

No: 171/2020

1st April 2020

For the Immediate Attention of All:

Postal Branches
Divisional Representatives
Area Distribution Representatives

Dear Colleagues

Coronavirus - Temporary Change Of Process For Logistic And Area Distribution ADC's In Cab Coaching In Line With The Business Policy On Social Distancing

Branches and representatives are informed that the department have been in discussions with the business in relation to temporary arrangements for processes performed by Logistic and Area Distribution Advanced Driver Coaches (ADC's) to allow assessments to take place while ensuring social distancing measures.

As part of this we have jointly reviewed the processes for agency assessments/induction and training during the Coronavirus pandemic, in order that the principle of 'one cab one person' is complied with to support adherence to social distancing.

The temporary changes will ensure that ADCs do not carry out 'in cab' coaching.

The areas affected by the temporary changes are:

- Agency assessments
- Post RTC training
- Skills training (that require in cab)

Attached for information are details of the temporary changes that are to be adopted:

- Annex A – Coronavirus Temporary Change in Process Document
- Annex B – Generic Agency Drivers' Manual

In addition to the above the department understand that the business are also currently creating video aids, which it is hoped will assist the induction process going forward.

Temporary Process For Agency Assessments

The process ensures that all drivers being offered by the agency must have been assessed and passed by a reputable national logistics/distribution provider within the last 12 months and a full review of tachograph history will be undertaken for each candidate. Should there be no available driving history for any candidate then they will not be considered for assessment.

Temporary Process For Post RTC Refresher

The process also includes interim measures for Post-Accident Interventions following RTA's. For minor RTA's the process will mirror the agency arrangements for in yard assessments, with any in-cab follow up to be concluded once restrictions have been lifted. Any 1-2-1 engagement will be conducted remotely ensuring social distancing.

For Serious or Multiple RTA's, where the temporary inability to conduct in-cab support could lead to a precautionary removal from driving the process now ensures that management must discuss the matter with the CWU Representative and share all relevant information prior to any decision being taken.

Temporary Process for Training for specialised skills

In addition to the above, where it is not possible to deliver in cab training it has been agreed that the skills register will be maintained, to ensure that specialist vehicles (110 DD) can be aligned to match the available skilled drivers.

While the above measures should provide meaningful workload for our ADC's, it is also understood that, in line with the National agreements for each function, they may be asked to drive to support the operation.

We understand these are unprecedented times and we have agreed this temporary change to processes and agency assessments to ensure that the ADC is able to carry out their roles safely in line with social distancing, whilst maintaining the business' ability to safely assess agency drivers.

The temporary change of process will be monitored and reviewed weekly at National level. The business will share with us the agency fall to ground and any changes required to this temporary process will be made Nationally, to ensure we maintain our ADC's safety whilst at work and consistency in the delivery of driver assessments.

Any enquiries in relation to this LTB should be addressed to Davie Robertson, Assistant Secretary, email: dwiyatt@cwu.org quoting reference 014.14

Yours sincerely

Davie Robertson
Assistant Secretary

Temporary Change of process for Logistic and Area Distribution ADC in Cab coaching in line with the business Policy on Social Distancing

Background

Due to Coronavirus we have had to review the process for agency assessments / induction and training so we comply the principle of 'one cab one person' and therefore ensuring the ADCs do not carry out 'in cab' coaching. This is a temporary measure given the current requirement to maximise social distancing in relation to the Coronavirus and will be reviewed on a regular basis.

The business can confirm they have a robust approach to how we are managing our flexible workforce in light of Covid-19 and this is very much in line with our RMG approach.

Areas effected:

- Agency assessments
- Post RTC training
- Skills training (that required in cab)

Temporary Process for Agency assessments

- The business will only Accept drivers who have been assessed and passed by a reputable national logistics / distribution operator within the last 12 months
- Full review of tachograph history (define time to be reviewed and outline standards required)
- Arrange in yard visual observation of slow manoeuvring driving activities to be carried out by the ADC
- Observe from a social distance key tasks eg coupling / uncoupling
- Induction tasks to be done by socially distant observation and mix of slides and prep of videos (This will be done by one standard slide pack agreed with the CWU for use at all induction's with some possible videos to support)

If there is no suitable driving and assessment history then no assessment will be given.

Temporary Process for Post RTC refresher

In case of 'minor' RTC (to be locally determined but supported by SHE Business Partner shared with the local CWU) only:

- In yard assessment as per agency
- 121 (socially distanced) scenario based review of RTC and key learnings
- Note to do in cab assessment as soon as practicable (post COVID 19)

Serious RTC or history of multiple RTCs removed to support in operation and in cab assessment as soon as practicable (again working in conjunction with SHE Business Partner and post COVID 19)- if consideration is being given to remove the driver from driving and to find work indoors to support the operation the unit managers must speak with the local CWU representative and share all relevant information used in making this decision prior to any decision being taken.

Temporary Process for Training for specialised skills

No in cab training to take place but will need a fully transparent skills list for all skilled roles eg 110DDTs- we will look to work flexibly to maximise the use of skilled resource where available and this may include the use of ADCs to support operational requirements, and potential movement of trailers where we have skilled, trained workers available.



Royal Mail Network

Agency Drivers Handbook



Royal Mail Fleet One Number 0345 2660005



Contact Numbers: main sites

Enter VOC name here	Enter VOC address here	Transport Office: Enter Phone numbers here
Enter other group sites names here	Address here	Tel: Enter number here
Enter other group sites names here	Address here	Tel: Enter number here
Central Postal Control (CPC)		Tel: 0345 2661060



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Safe Systems of Work (SSoW) some are Site specific

All task and procedures in Royal Mail are covered by a Safe Systems of Work.

When performing any of the tasks or procedures you must ensure you follow the correct SSoW copies of all the relevant SSoW to your work role are located in the Transport Office on your site. If you are asked to perform a task or procedure you have not been trained or given a SSoW for please see your Line manager who should be able to give you the required information or instruction as what is needed.



Health and Safety Brief for Agency Drivers

To comply with our 'O' Licence maintenance obligations (that affect all vehicles, and not just those 'O' Licensed), Royal Mail has committed to maintain its vehicle fleet to a standard agreed with the Department of Transport.

In addition, duty of care to our people and other road users and members of the public, as well as the Health & Safety at Work Act, means that Royal Mail must ensure all drivers are aware of their responsibilities.

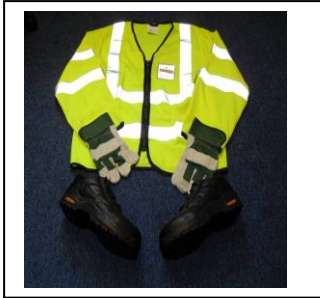
To maintain vehicles in a roadworthy condition, it is the legal responsibility of every driver to carry out basic safety and maintenance checks every time a vehicle is used.

Drivers working in Royal Mail Operations must undertake different levels of safety check at different times. Checks when a vehicle is first used (daily or first use check), at the start of another duty (duty check), and weekly ones vary slightly, and drivers must understand which is to be done and when. Managers must also carry out periodic checks of their own to make sure vehicles are safe.

Your manager must ensure vehicles are presented to the maintaining workshop for routine inspection/servicing and repairs, on time and in line with an appointment agreed with the Vehicle Services garage network.

Royal Mail provides the necessary support, information and training for drivers on: health and safety; safe systems of work; safe loading; manual handling; vehicle pre-use safety checks; the PMT1 Fault Reporting procedure; security procedures; the Road Traffic Collision (RTC) procedure; and professional driver skills training (where appropriate). All training will be recorded in a driver's individual record.

PPE Required



- Your Agency should supply PPE
- Photo I D Card
- Yellow Class 3 Hi-Viz (Long Sleeve)
- Gloves
- A standard issue uniform
- Steel Toe cap shoes/boots are required on Royal Mail Sites

Digital Tachograph Policy

Agency Driver card download frequency

- Driver cards must be downloaded **on a daily basis** at the start and end of the individuals' duty, as per local requirements, when the individuals collect and return their vehicle keys from the traffic office.

Manual entries

- Individuals must manually input any 'other work', 'breaks' for periods where the driver card has been removed. All manual entries must be made at the time of first insertion of the driver card at the start of each period of digital tachograph driving.
- Any work recorded on an analogue tachograph **must not** be recorded on the digital tachograph, as this would constitute double accounting.
- It is the individuals' responsibility to report any omitted manual entries via a print out from the vehicle. Two print outs should be taken and any manual entries, entered on the back of the print out, and signed and dated by the individual. One copy must be handed to the traffic office and the other retained by the driver for the necessary retention period.
- Duplicate print outs must be handed to your manager to enable amendments to be made to your driving records on the drivers' hours' software system.
- Any such amendments will not be transferred back on to the drivers' card, so care must be taken to ensure manual entries are made at the correct time.
- It is recommended that an individual makes a note of hours worked at the end of the previous day, in order to complete the Digital Tachograph manual record at the start of the following period of driving. This is particularly important if the individual is going on leave or when not driving a digital tachograph vehicle for a period of time, to ensure details are not forgotten.
- Driver's must complete all manual entries accurately. Inaccurate entries are a prosecutable offence and could also result in action in line with the RMG conduct code.
(Please See Appendix for Manual Entry)



EU / RTD Golden Rules

● EU Hours

- You must take a break after 4½ hours of continuous driving.
- You can normally drive for only 9 hours a day you can extend this to 10 hours, twice a week.
- Under EU hours the 45 minute can be split into two periods, first must be a minimum 15 minutes, second a minimum 30 minutes.
- On any day you drive under EU Hours you must have your daily rest (11 hours, or reduced to 9 up to 3 times in a week).
- On any week in which you drive under EU Hours, you must have your weekly rest (45 hours, or it may be reduced to a minimum of 24 hours). This must be paid back en-bloc by the end of the 3rd week following.
- Weekly driving is limited to 56 hours.
- Driver must take a weekly rest after a maximum of 6 daily driving periods.
- Driver must not exceed 90 hours driving time in any rolling fortnight
- You MUST declare paid work done for another employer (or self employed).

● RTD

- You can only WORK for a maximum of 60 hours in a week, which does not include breaks.
- You can only work for an average of 48 hours over a reference period (26 weeks).
- Twenty days annual leave (including BH) can't be used to reduce average working time.
- A week is the period between 00:00 on any Monday and 24:00 on the following Sunday
- A worker may not work for more than 6 hours without a break. Breaks must be at least 15 minutes long. A total of 30 minutes of break is needed if total working time is over 6 but not over 9 hours, or 45 minute if over 9 hours.
- All night workers are entitled to an annual free health assessment.
- EU drivers hours rules with regards to break and rest periods take precedence at all times
- It is the employees responsibility to inform their main employer of any other work they might be doing.



Manual Entries

As part of the Royal Mails 'O' Licence obligations, drivers are required to provide an honest account of their working day

- Insert your 'Driver Card' into the tachograph unit on arrival at your vehicle.
- Do Not use POA. You only need to show Other Work/Driving or Rest.
- Complete a Manual Entry for the end of your previous shift to cover the period from removing your card from your vehicle and leaving site. This should be shown as 'Other Work'.
- Show daily rest using the 'Bed' symbol.
- Show your duty start time at the start of your shift.
- Your Tachograph record must match the attendance sheet.
- If changing vehicles during your duty you will need to make a manual entry to cover the period between vehicles.
- You must download your card each day at the Start and Finish of your duty.

When undertaking other duties including; Shunting or training, then you must ensure all activity is recorded on your Digital Driver Card.

Siemens VDO 1.4

Manual Entries

Example shown - Working away from the vehicle without the driver card inserted

Driver card was inserted on 14/11/11 at 14:00 local time with the following manual entries:

- Adding 55 mins ⚡ on to the end of the shift 05/11/11 from 17:50 to 18:45 Local Time
- Prefixing 1hr 50 mins ⚡ to the start of the shift 14/11/11 from 12:10 to 14:00 Local Time

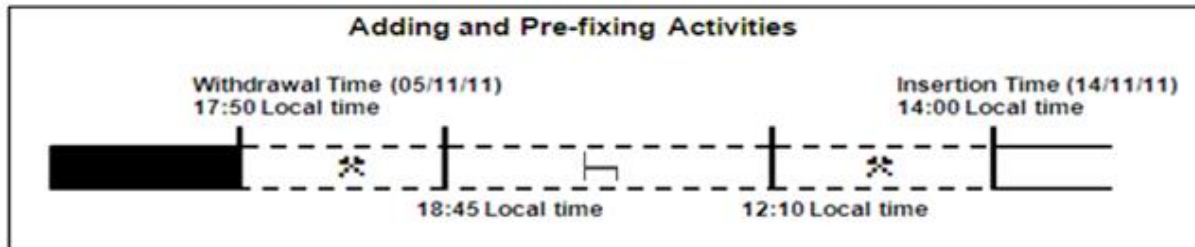


Table - A summary of the EU drivers' hours rules and sector specific working time rules

Drivers' hours rules Regulation (EC)561/2006	Working time rules Directive 2002/15/EC
Driving 9 hour daily driving limit (can be increased to 10 hours twice a week) - Maximum 56 hour weekly driving limit - Maximum 90 hour fortnightly driving limit	Working time (including driving) - Working time must not exceed average of 48 hours a week (no opt out)¹ - Maximum working time of 60 hours in one week (provided average not exceeded) - Maximum working time of 10 hours if night work performed²
Breaks 45 minutes break after 4.5 hours driving - A break can be split into two periods, the first being at least 15 minutes and the second at least 30 minutes (which must be completed after 4.5 hours driving)	Breaks³ - Cannot work for more than 6 hours without a break. A break should be at least 15 minutes long 30 minute break if working between 6 and 9 hours in total⁴ 45 minute break if working more than 9 hours in total
Rest 11 hour daily rest; which can be reduced to 9 hours no more than three times a week (or split into 3 hours + 9 hours as often as desired) 45 hours weekly rest, which can be reduced to 24 hours, provided at least one full rest is taken in any fortnight. There should be no more than six consecutive 24 hour periods between weekly rests.	Rest Same rest requirements as EU drivers' hours rules

¹ Normally calculated over a rolling 17 week period, but can be extended to 26 weeks under a collective or workforce agreement

² Can be extended under a collective or workforce agreement

³ EC Regulation 561/2006 is directly effective and takes precedence over EC Directive 2002/15 - Article 2.4 Directive 2002/15. Therefore, EU drivers' hours break requirements take precedence when driving

⁴ After working for 6 hours a mobile worker must take a break of at least 15 minutes. However, if working more than 6 and up to 9 hours in a shift a mobile worker needs to take a break totalling at least 30 minutes - this could be two breaks of 15 minutes. Where a shift will contain more than 9 hours of working time, a total of 45 minutes of break is needed.



Royal Mail Group Internal Information

RMG Security Rules for Drivers - Logistics

PLEASE READ THIS DOCUMENT CAREFULLY AND THEN SIGN THE DECLARATION

Before using your vehicle/trailer

- Ensure that all door locks work and windows operate effectively
- If you discover a fault with your vehicle/trailers security, report it to your manager immediately

Security Rules

- When parking and leaving your cab/trailer at a trailer exchange point, customer premises or other authorised non-RMG site, ensure all cab windows are fully closed and all cab doors are securely locked. Wherever possible, park in a well-lit and busy area.
- When making scheduled / unscheduled stops at a non RMG site which results in leaving the vehicle and trailer for any period of time (including for comfort breaks or scheduled breaks / meal reliefs taken at motorway services and other similar truck stops), the driver is required to ensure all cab windows are fully closed and all cab doors are securely locked. Also, the trailer unit should be secured with a seal. This will enable the driver to be confident that the trailer has not been tampered with whilst they were away from it. A seal has been provided for this purpose and is contained within the drivers pack. The seal will be used on the rear shutter lock mechanism before departing from the vehicle.
- On return to your vehicle, carry out a visual check around the vehicle / trailer for any signs of intrusion or tampering. If your vehicle has been issued with a trailer seal (for example, a vehicle carrying screened air mail) check that the seal is unbroken. Should you discover any such signs, please report it to your office manager or Central Postal Control immediately.
- Keep to your scheduled route and never carry unauthorised passengers
- Only stop where you are required to do so to perform your duty. Be extremely wary of accidents, broken-down vehicles, people trying to flag you down, etc.
- When parking at a RMG site, ensure you close all cab windows and lock both doors
- Vehicle keys should remain with the driver at all times. The only exception is when Health & Safety arrangements exist at RMG or certain customer sites that require that vehicle keys must be handed over to designated staff prior to loading/unloading of trailers



Yard Safety Standard

Our biggest hazard is workplace transport. Our yards are busy places and in a collision between a vehicle and a human the person will always come off worse.

These are the rules

- ONLY those with legitimate business are authorised to be in an operational yard
- Unless specifically authorised, indoor employees are NOT allowed in the operational yard
- Authorised persons MUST wear class 3 high visibility clothing (fastened) at all times when in the yard. Class 3 is typified by long sleeves rather than waistcoat and in Logistics is usually yellow in colour
- Where Logistics control the yard (RDC's) all visiting drivers MUST comply with the class 3 rule.
- The individual is responsible for ensuring that high visibility clothing is kept clean and in a good state of repair
- Pedestrians must use the routes marked. Where there is a smoking area on site the official walk route MUST be followed at all times
- Infringements of these rules must be brought to the attention of a manager at the earliest opportunity

Coupling and uncoupling of trailers/bay docking safety standard

This is a complex activity that must only be carried out by those trained to do so. Safe Systems of Work (SSOW) have been developed; your manager will be able to provide you copies. The key messages are extracted below.

These are the rules

- ALL drivers MUST abide by the applicable safe systems of work for coupling/uncoupling, Susie lock removal and bay docking at all times
- ONLY drivers can couple or uncouple trailers
- When Susie locks are in use to secure trailers, the trailer lock off SSOW MUST be followed at all times, this includes parking the Motive Unit at a right angle to the trailer bulkhead before removing or refitting the lock
- ONLY a driver or a Workshop Technician can fit or remove a Susie lock
- All drivers MUST exit the Motive Unit cab and make a full visual observation of the area between the motive unit and the trailer before coupling to check if any persons are present; this should be done from the side of the vehicle cab
- Shunter Unit drivers may make this check without alighting from the vehicle

	The site speed limit is 10mph.			Class 3 High Visibility jackets must be worn in the yard at all times.
	Safety Footwear Must be Worn			No Photo ID No Entry
	Smoking only allowed in designated areas			Use crossings and walkways at all times
	Trailer Brakes must be applied on all dropped trailers			If the fire alarm sounds leave your vehicle and go to a fire assembly point
	Do not use mobile phones whilst driving			Do not park in restricted areas

 **Welcome to the Atherstone Parcel Sort Centre** 
Revised Yard Layout for 2018
Drive Carefully - Beware of Pinch Points around the Yard

Reversing in the Yard

If in doubt Get Out And Look

Loading Bay Doors
Truck must be left on the bay door control box before it can be opened

Connecting to a Trailer

Pulling Away

Only connect to a trailer or pull off a bay with a **GREEN** light showing.

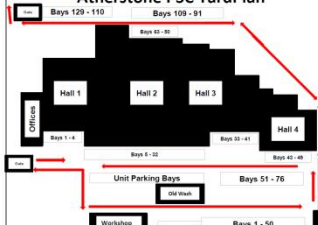
Atherstone Site Rules Pictorial v3 141118.j65

Page 1

Atherstone Parcel Sorting Centre - Site Rules

	The site speed limit is 10mph.			Class 3 High Visibility jackets must be worn in the yard at all times.
	Safety Footwear Must be Worn			No Photo ID No Entry
	Smoking only allowed in designated areas			Use crossings and walkways at all times
	Trailer Brakes must be applied on all dropped trailers			If the fire alarm sounds leave your vehicle and go to a fire assembly point
	Do not use mobile phones whilst driving			Do not park in restricted areas

Atherstone PSC YardPlan



Map labels include: Bays 120-110, Bays 109-91, Bays 80-60, Bays 50-40, Bays 30-20, Bays 1-50, Bays 77-88, Bays 51-76, Bays 1-10, Bays 11-20, Bays 21-30, Bays 31-40, Bays 41-50, Bays 51-60, Bays 61-70, Bays 71-80, Bays 81-90, Bays 91-100, Bays 101-110, Bays 111-120, Bays 121-130, Bays 131-140, Bays 141-150, Bays 151-160, Bays 161-170, Bays 171-180, Bays 181-190, Bays 191-200, Bays 201-210, Bays 211-220, Bays 221-230, Bays 231-240, Bays 241-250, Bays 251-260, Bays 261-270, Bays 271-280, Bays 281-290, Bays 291-300, Bays 301-310, Bays 311-320, Bays 321-330, Bays 331-340, Bays 341-350, Bays 351-360, Bays 361-370, Bays 371-380, Bays 381-390, Bays 391-400, Bays 401-410, Bays 411-420, Bays 421-430, Bays 431-440, Bays 441-450, Bays 451-460, Bays 461-470, Bays 471-480, Bays 481-490, Bays 491-500, Bays 501-510, Bays 511-520, Bays 521-530, Bays 531-540, Bays 541-550, Bays 551-560, Bays 561-570, Bays 571-580, Bays 581-590, Bays 591-600, Bays 601-610, Bays 611-620, Bays 621-630, Bays 631-640, Bays 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1931-1940, Bays 1941-1950, Bays 1951-1960, Bays 1961-1970, Bays 1971-1980, Bays 1981-1990, Bays 1991-2000, Bays 2001-2010, Bays 2011-2020, Bays 2021-2030, Bays 2031-2040, Bays 2041-2050, Bays 2051-2060, Bays 2061-2070, Bays 2071-2080, Bays 2081-2090, Bays 2091-2100, Bays 2101-2110, Bays 2111-2120, Bays 2121-2130, Bays 2131-2140, Bays 2141-2150, Bays 2151-2160, Bays 2161-2170, Bays 2171-2180, Bays 2181-2190, Bays 2191-2200, Bays 2201-2210, Bays 2211-2220, Bays 2221-2230, Bays 2231-2240, Bays 2241-2250, Bays 2251-2260, Bays 2261-2270, Bays 2271-2280, Bays 2281-2290, Bays 2291-2300, Bays 2301-2310, Bays 2311-2320, Bays 2321-2330, Bays 2331-2340, Bays 2341-2350, Bays 2351-2360, Bays 2361-2370, Bays 2371-2380, Bays 2381-2390, Bays 2391-2400, Bays 2401-2410, Bays 2411-2420, Bays 2421-2430, Bays 2431-2440, Bays 2441-2450, Bays 2451-2460, Bays 2461-2470, Bays 2471-2480, Bays 2481-2490, Bays 2491-2500, Bays 2501-2510, Bays 2511-2520, Bays 2521-2530, Bays 2531-2540, Bays 2541-2550, Bays 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3171-3180, Bays 3181-3190, Bays 3191-3200, Bays 3201-3210, Bays 3211-3220, Bays 3221-3230, Bays 3231-3240, Bays 3241-3250, Bays 3251-3260, Bays 3261-3270, Bays 3271-3280, Bays 3281-3290, Bays 3291-3300, Bays 3301-3310, Bays 3311-3320, Bays 3321-3330, Bays 3331-3340, Bays 3341-3350, Bays 3351-3360, Bays 3361-3370, Bays 3371-3380, Bays 3381-3390, Bays 3391-3400, Bays 3401-3410, Bays 3411-3420, Bays 3421-3430, Bays 3431-3440, Bays 3441-3450, Bays 3451-3460, Bays 3461-3470, Bays 3471-3480, Bays 3481-3490, Bays 3491-3500, Bays 3501-3510, Bays 3511-3520, Bays 3521-3530, Bays 3531-3540, Bays 3541-3550, Bays 3551-3560, Bays 3561-3570, Bays 3571-3580, Bays 3581-3590, Bays 3591-3600, Bays 3601-3610, Bays 3611-3620, Bays 3621-3630, Bays 3631-3640, Bays 3641-3650, Bays 3651-3660, Bays 3661-3670, Bays 3671-3680, Bays 3681-3690, Bays 3691-3700, Bays 3701-3710, Bays 3711-3720, Bays 3721-3730, Bays 3731-3740, Bays 3741-3750, Bays 3751-3760, Bays 3761-3770, Bays 3771-3780, Bays 3781-3790, Bays 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8131-8140, Bays 8141-8150, Bays 8151-8160, Bays 8161-8170, Bays 8171-8180, Bays 8181-8190, Bays 8191-8200, Bays 8201-8210, Bays 8211-8220, Bays 822

Parcelforce National & International Hub

On site rules for drivers and vehicles



**One way system
except for shunt or
marshall vehicles**



**Class 3 Hi Vis vest
and safety
footwear is
mandatory**



**Crossing
pedestrians have
right of way**



**All vehicles to be
reversed into
parking bays**



**Apply brake when
trailer is parked**



**Keep to the 15
mph speed limit at
all times**

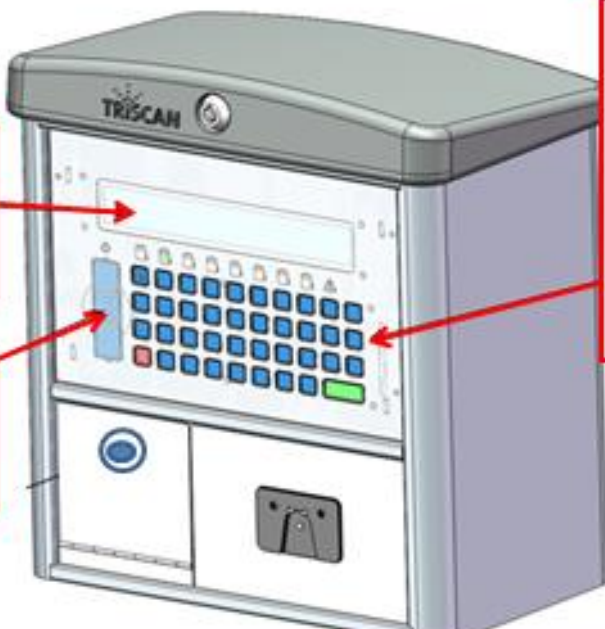


**Switch off engine
and remove key
from ignition when
parked**

Refuelling Policy



Introducing your new Apollo fuel unit



1. SHOW VEHICLE FOB TO READER - YOU ONLY NEED TO DO THIS ONCE

2. WHEN THE DISPLAY SHOWS "AUTHORISING" REMOVE THE FOB FROM THE READER

3. ENTER THE ODO READING USING THE KEYPAD

IF ODO READING IS REJECTED, YOU DON'T NEED TO SHOW FOB AGAIN JUST RE-ENTER THE CORRECT ODO READING

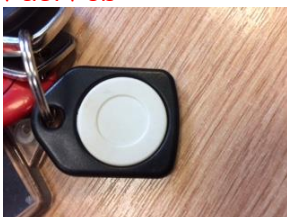
4. ENTER PUMP NUMBER IF PROMPTED

FUELLING MUST COMMENCE WITHIN 30 SECONDS OR THE PROCESS WILL RESTART

KEY POINTS:

- DO NOT REMOVE NOZZLE FROM THE PUMP UNTIL BOTH FOB AND ODO READING HAS BEEN ACCEPTED**
- YOU ONLY HAVE TO SHOW THE FOB TO THE READER ONCE - UNLESS THE SYSTEM TIMES OUT.**
- REMOVE FOB FROM THE UNIT WHEN THE DISPLAY SHOWS 'AUTHORISING'**
- HGV VEHICLES WITH TELEMETRY ARE NOT REQUIRED TO ENTER ODO**

Fuel Fob





Office Administration – Drivers Checks

- All ways sign on at the correct time at the start of your duty.
- Sign for your Vehicle Pack, and pick up your drivers duty pack.
- *Proceed to Yard Area on approach to your vehicle:-*
- Check the general condition of the vehicle for damage and report any fresh damage to a line manager before you move the vehicle.
- Check vehicle warning systems
- Make your manual entries for your Digital Tacho
- Vehicle checks (Interior).
- Vehicle checks (under Bonnet).
- Vehicle checks (Walk around).
- *You must show your vehicle check time on your digi card, DVSA will want to see this should you be stopped for a road side check.*

Duty checks The items listed below must be checked prior to commencing any driving duty. Where a vehicle is to be taken onto the road for the first time in a day, checks should be conducted in addition to daily and weekly checks, as directed. Duty checks (inside the cab) • Completion of logbook • Seat belts (operation of) • Fuel level (ensure sufficient for duty) • Brake pedal movement/operation (static check) • Handbrake movement/operation (static check) • Horn (operation of) but consider operating times • Operation of windscreen wipers and washers • Steering (free play) • Cleanliness of vehicle and windows (inside) • Operation of heater/demister • Dashboard warning lights. • Oil level. • Coolant level.	Duty checks (walking around the vehicle) • Condition/cleanliness of all glass, including mirrors • Lights and lenses (operation, condition and security) • Doors (security) • Tyres' visual check (pressure, tread and sidewall) • Vehicle excise duty (correctly displayed and in date) • 'No Smoking' sign (correctly displayed and of the correct type for your office) • Visual inspection of wheel nuts and indicators (where fitted) • Condition of number plate bodywork and attachments • Cleanliness of vehicle (outside) • Signs of oil/coolant/fluid leaks (report on PMT1 if anything found) • Load, including security and condition of restraint straps/system • Operation and security of tail lift (where fitted). Additional duty checks for LGVs • Trailer connections (suzies) • Fifth-wheel connection and safety chain • 'O' Licence disc displayed • Trailer tail lift power lead (where fitted) • Trailer lights • Trailer number plate • Trailer leg stowage • Vehicle height displayed/set in cab • Side guard and under run protection equipment (>3.5 tonne vehicles) • Wheel torque indicators (>3.5 tonne vehicles).
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APPENDIX 3

BRVC206

ROYAL MAIL P318 DUTY SHEET

**BRVC20
6**
Location : BENDVOC
Duty Number : BRVC206
Duty Holder :
1st Vehicle Type : Double Deck MU
2nd Vehicle Type :
3rd Vehicle Type :
Amendment Date : 10/10/11

	Attendance		M/R Total	Work Time Learnt	
	From	To		From	To
Mon					
Tue	12:17	09:35	01:30		
Wed	12:17	09:35	01:30		
Thu	12:17	09:35	01:30		
Fri	12:17	09:35	01:30		
Sat					
Sun					

WORK AS DIRECTED BY THE SUPERVISING OFFICER OR AS DETAILED BELOW.

Times		Duty Details	CAB I.D.	Special Instructions	RCS Subheads	Time Occupied		Daily Driving Time
From	To					Daily	Weekly	
12:17	12:42	Start Facility BRIDGEND VOC			D01	00:25	01:40	
12:42	13:12	Travel CARDIFF MC Empty	BRVC206	Must be travelling	M05	00:30	02:00	00:30
13:12	13:42	Meal Relief CARDIFF MC			N00	00:30	02:00	
13:42	14:12	Load/Assist CARDIFF MC			M05	00:30	02:00	
14:12	17:35	Travel NATIONAL DISTRIBUTION CENTRE STL (Includes M51) CF.NDC.2	BRVC206	Must be travelling	M05	03:10	13:12	03:10
17:35	18:05	Unload/Assist NATIONAL DISTRIBUTION CENTRE			M05	00:30	02:00	
18:05	18:05	Drop Trailer NATIONAL DISTRIBUTION CENTRE			M05	00:05	09:20	
18:05	18:05	Meal Relief Whilst Vehicle Un/Loaded NATIONAL DISTRIBUTION CENTRE			N00	00:45	03:00	
18:50	19:05	Hook Up To Trailer NATIONAL DISTRIBUTION CENTRE			M05	00:10	09:40	
19:05	19:05	As Directed NATIONAL DISTRIBUTION CENTRE			M01	00:05	00:20	
19:05	22:45	Travel SWANSEA MC STL (Includes M51) NDC.SA	BRVC206	Must be travelling	M05	03:40	14:40	03:40
22:45	23:15	Unload/Assist SWANSEA MC			M05	00:30	02:50	
23:15	23:20	Refuel SWANSEA MC			D01	00:05	00:20	
23:20	23:35	Meal Relief SWANSEA MC			N00	00:15	01:00	
23:35	00:20	Travel BRIDGEND VOC Empty	BRVC206	Must be travelling	M05	00:45	03:00	00:45
00:20	00:35	End Facility BRIDGEND VOC			D01	00:15	01:00	
						12:18	49:12	00:13

Uniforms must be worn at all times, this includes totector shoes and reflective jackets/tabbards.

All staff must observe the Health & Safety rules at all times.

All drivers will refuel their vehicles at the end of the duty. If they are unable to do so, this should be reported to the shift manager.

In case of emergency or query, please contact the Distribution Shift Manager.

Empty containers to be moved as requested by CPC

Spore



If you are involved in an RTC or have an Accident on Duty it has to be reported to CPC (use the number on the front of the handbook). Personal accidents need to be reported at sites where they occur or on return to office if not on RM premises.

1. If you have a P5772, (this is an A5 card for keeping in the Royal Mail vehicle to assist in gathering the other parties information, and to provide them with our details) in your vehicle then complete this and hand Part 2 to the other party.
- Name and Address of the driver of the other vehicle.
 - Name and Address of the owner of the other vehicle (if different).
 - The insurance details of the other driver, if they know them or have them to hand, don't delay reporting the incident if you can't obtain these.
 - The other vehicle's registration number, make and model.
 - If there are more than one other vehicle involved, obtain the above for all the driver's and vehicles.
 - If there are too many vehicles involved to obtain details for all vehicles, please obtain the details of the people who collided with you, or who you collided with.
 - If a Police Officer attends the scene then obtain their details
 - Take photographs, if safe to do so.

-Name of Officer

-Police Officers number

-Name and address of station

-Station phone number

You will also need to make a note of the following at the scene to submit to your Traffic Office:

- Name and address of any witnesses.
- Exact location of the accident.
- Time of the accident.
- If the other vehicle has passengers on board, if possible, obtain their details. If not possible then make a note of the number of passengers in each vehicle.
- Length of any tyre abrasions by either party.
- Positions of the vehicles after the collision.
- Any road markings or road signs.

People involved in an RTC must sit down with a Traffic Office manager to record the details as soon as they return to the office, otherwise what happened leading up to the collision will become unclear in your mind. This information should be captured and held locally, in case you need to refer to it later (e.g. for legal or performance review purposes).

The Manager then needs to ensure all the details of the RTC and any injuries are entered onto the [Accident reporting system](#) (and other reporting documents and management summaries).



Royal Mail Fleet One Number



Fleet

Delivering first class fleet services

0345 2660005

- Breakdown
- Accident
- Vehicle Maintenance Bookings
- Tyres
- Glass
- Fuel
- In-life team

What you will hear when you call Royal Mail Fleet One Number:

Welcome to Royal Mail Fleet. Please note we record and monitor calls for training purposes, service quality and our mutual protection.

Option 1: Breakdown

Option 2: Accident

Option 3: For vehicle maintenance bookings

Option 4: Tyres

Option 5: Glass

Option 6: Fuel and In Life team



Driver Responsibilities



Seat Belts

Seat belts must be worn at all times while driving.

Seat belts are fitted to the front seats of all cars, light vans and large goods vehicles manufactured after October 2001, and where fitted they must be worn.

Seat belts are also required in the rear of passenger-carrying vehicles, and in most cases, the driver is responsible for ensuring they are worn.

Seat belts should operate freely, and should not be damaged or frayed. Seat belt locks should open and release without difficulty, and drivers should ensure all seat belts fitted in their vehicle operate correctly, as part of their normal duty checks.



Smoking in vehicles

The law on smoking in public places has changed and it is now an offence throughout the UK to smoke in any enclosed space wherever other people work.

Royal Mail does not therefore allow smoking on any official premises and this extends to vehicles.

Drivers are responsible for the vehicle/s they drive and must ensure an appropriate 'No Smoking' sign is fitted in the correct place inside their vehicle. This should be included as one of your regular driver checks. Royal Mail's smoking policy has been developed to take into account the health of those team members that may be affected by passive smoking (ie. the effects from other people's smoking), as well as considering workplace legislation and a general duty of care owed to all our people. Ask your manager for more details.

Contravention of the smoking policy is a serious offence, so never smoke in a vehicle, whether driving or as a passenger. The penalty for smoking in a vehicle could be an on the spot fine, payable by the driver. Smoking could be dangerous in itself, as it could result in a serious fire if the cigarette or hot ash was dropped. Smoking while driving could cause an accident, as it detracts from concentration. Smoking while refuelling a vehicle is extremely dangerous and must never occur, as fuel (including diesel) is extremely flammable under certain conditions and a resulting fire is very hard to extinguish without the right equipment and training.

Above all, smoking is dangerous to your health and the health of those around you.

Never smoke while on duty, other than in designated smoking areas. You will be personally liable if caught breaking the law and you will be responsible for any resulting fine imposed.

E- Cigarettes are treated as normal cigarettes by Royal Mail.

Work Time Listening and Learning Brief



Driving and mobile phones

April 2016

Intended Audience: Drivers

Managers' Brief

Introduction

Mobile phones are an essential part of everyday life – both for personal and business use. However, as their popularity has increased, so have the risks for you and others when they are used on the road. It's been proven that both handheld and hands-free mobile phones have an adverse effect on driver performance.

The law prohibits the use of handheld mobile phones and any other communications devices while driving. It is also against business policy.

Overview

- Research shows that drivers using a mobile phone are four times more likely to be in a road traffic collision. Taking or making a call, even on hands-free, increases the risk significantly because full concentration can't be given. The driver is also likely to be less prepared for an emergency if one were to happen
- The law also relates to mobile phone use while texting and using other smart phone functions
- The fixed penalty fine for illegal use of a mobile phone whilst driving is £100, and three penalty points. If the case goes to court, you could be disqualified from driving and be fined up to £1,000
- Any prosecutions that take place as a result are the sole responsibility of the driver. We will not pay any fines on their behalf
- In addition, the driver could also face consequences under Royal Mail's conduct code.

Actions

- While driving, if you need to make or receive a call, read or write a text, or use a function on your smart phone, stop in a safe place before doing so
- Mobile phones should be switched off until the vehicle is stopped and parked safely. If you drive regularly, you might want to record a voice mail message that says you are driving and will reply when it's safe to do so
- Although it's perceived that handheld phones are more dangerous, research shows that using hands free phones or devices does not significantly reduce the risk. This is because the driver is still distracted and their attention is split between the conversation and driving. The person on the phone also can't see the potential hazards that the driver may be facing
- If you're in a filling station – even if your vehicle is not being refuelled – do not use your mobile phone. Flammable or explosive materials, such as petrol, must not be stored or carried in the same compartment of the vehicle as a mobile phone.

Access & Egress for Vehicles (Royal Mail Network)

This safe system of work must be followed at all times and consolidates advice given in operator training. Any problems should be reported to your line manager immediately.

It is the legal responsibility of every employee at work:

- To take reasonable care for the health and safety of themselves and others who may be affected by their acts or omissions.
- To report defective or faulty equipment immediately.
- To wear the appropriate PPE identified for the task and to ensure its fit for purpose.

This policy should be the definitive reference, but must be considered when performing site specific Risk Assessments/SSoW.

To comply with the Work at Height Regulations 2005 it is best practice to adopt the '**3 points of contact**' principle (there should always be either both hands and one foot or one hand and both feet in contact with the steps or handholds) where applicable when gaining Access to or Egress from vehicles. In some circumstances the application of the '**3 points of contact**' stance doesn't demonstrate good body posture as per Royal Mail manual handling training.

Before use

Check for defects. If any defects are found follow Royal Mail's fault reporting procedure - PMT1.

- Check the security of grab handles.
- Check security and condition of vehicle steps and ladders.



Access

Check to ensure that the ground and the surrounding area is suitable for safe access before firmly gripping the grab handles provided.

Steady your body as you place your foot on the bottom step. Be sure to check the integrity of the step for any structural damage or defects, before placing your full body weight on it. Raise your body up using the grab handles so that you can climb into the cab, using the grab handles simultaneously placing your feet on the steps. Once you are stood on the cab floor ease your body into the vehicle seat.

Egress

Check to ensure that the ground and the surrounding area is suitable for safe egress before preparing to descend from the vehicle. Under no circumstances should you jump or egress in a forward manner from the cab of a vehicle. Hold the front pillar grab handle whilst easing your body out of the seat to place your right foot on the top step. Then take hold of the left pillar grab handle and step onto the next step with your left foot. Repeat this process until you step onto the ground supporting yourself using the grab handles as you go.



Drivers Vehicle Defect Reporting **Instructions On Use.**

Drivers/Users Responsibility:-

Enter the following details:-

1. Serial number of vehicle with a defect.
2. Odometer reading – identifying whether miles or kilometres.
3. Depot/Operating centre name. This is the name of the location where the fault has been identified and where the P(F)MT1 has been raised.
4. Date when the P(F)MT1 was raised.
5. Time of completion (24 hour clock).
6. A short description of the defects(s).
7. Driver's signature.
8. Print name.

REMEMBER – ALL DEFECTS MUST BE RECORDED IN THE VEHICLE CONDITION LOG BOOK

9. Pass completed form and Vehicle Condition Log Book to traffic office - officer in charge.

Line Managers Responsibility:-

1. To check that the entry has been made in the vehicle condition log book.

Enter the following details on the defect report

2. Signature to acknowledge receipt.
3. Date received.
4. Time received (24 hour clock).
5. Complete details in Vehicle Defect Control Register (back cover of PFMT1 or Manual Register where PMT1 is used).
6. Contact workshop to arrange for rectification of the defect(s).

Enter the following details on the Defect Report.

7. Enter date and time that workshop was notified (24 hour clock).
8. Enter date and time when workshop presented with vehicle (24 hour clock).

Pass Vehicle Condition Log Book and copies A and B of the Defect Report to workshop

Workshops Responsibility:-

1. Checks to see if vehicle/trailer is due scheduled work.
2. Arrange defect/scheduled work due to be completed.

When defect has been cleared enter the following details on both copies

3. Signature of person who completed the repair to acknowledge defect(s) has/have been rectified.
4. Grade of signing person.
5. Date.
6. Clears defect(s) in Vehicle Condition Log Book, passes Vehicle Defect Report and Vehicle Condition Log Book to LTT.

Workshop Manager's/LTT Responsibility.

7. Delete as appropriate the repair classifications that are NOT appropriate to the defect.

Contact the operator to inform them that the defect/work is rectified.

ENTER THE FOLLOWING DETAILS ON THE DEFECT REPORT.

8. Date operator notified.
9. Time operator notified (24 hour clock).

Pass completed Defect Report and Vehicle Condition Log Book to the operator after entering the following details on both copies of the Defect Report.

10. Sign to validate that the Vehicle Report 'B' copy, and Vehicle Condition Log Book have been returned to the operator.
11. Date returned.
12. File 'A' copy of the Vehicle Defect Report in the maintenance file for that vehicle.

Depot/Operating Centre Officer In Charge Responsibility:-

Enter the following details in the Vehicle Defect Report Control Register.

1. Date when vehicle returned to service.
2. Date Vehicle Defect Report 'B' copy returned by workshop and initial entry.
3. Calculate the period of time vehicle was out of service.

File Vehicle Defect Report 'B' copy, with inspection sheet when it arrives, in vehicle file for 15 months.

Illustration of Log book entry and a PMT 1 also see appendix

Example of Log Book completion for a Vehicle due for a Duty Check with no Defects found.										
Vehicle Condition & Mileage Log					Vehicle Condition & Mileage Log					
Date	Duty	Vehicle Checks (Tick applicable)	Checks completed (Please sign)	Duty/Hel Special Trip	Old reading Start	Old reading finish	Defects found/nil defects	PMT/Ha	Manager	
24/07/2002	✓		Driver Initials <i>drb</i>	321	1234567	1234765	Nil			
Tick the appropriate box to indicate which checks have been carried out. This is NOT for the Duty number you are performing.					Enter your Duty number or a special Trip code number.					Only NIL should be entered here, unless a fault is found on the vehicle.

Example of Log Book completion for a Vehicle due for a Duty Check with a Defect found.										
Vehicle Condition & Mileage Log					Vehicle Condition & Mileage Log					
Date	Duty	Vehicle Checks (Tick applicable)	Checks completed (Please sign)	Duty/Hel Special Trip	Old reading Start	Old reading finish	Defects found/nil defects	PMT/Ha	Manager	
24/07/2002	✓		Driver Initials <i>drb</i>	321	1234567	1234765	Clutch fault	12345		
In this example the vehicle was due for a Duty Check. The person using the vehicle MUST perform the Duty Check as per the instructions in the Log Book. A Tick should then be entered to indicate the Check was carried out.					In this example a fault occurred on the vehicle. A brief description should be entered here.					The PMT1 number MUST be entered here.
					These columns are for Garage use only.					This is for the Unit Manager to sign when the pink copy of the PMT1 is received.

Note: The workshop must sign the fault off, not use a stamp to clear the fault. The fault should be signed off on the line that the entry has been made. If the workshop staff take the vehicle on the road they need to complete a full entry on the next available line in the logbook.



Completion of Fault Reports PMT1

The Fault Report (PMT1 or PFMT1) is an integral element of a vehicles maintenance records. Each selection of the document must be completed. N/A can be entered where appropriate.

- When completing the form the operator is responsible for entering the details of the vehicle.
- Tick the box with the appropriate fault area
- Enter the details of the fault

Vehicle Defects Work Request

Found on the Vehicle → **Serial No.**

Registration Number → **Registration No.**

Current mileage → **Odometer Reading**

Where you left the vehicle → **Location**

Date and time → **Time**

Full Description → **Description of Defect etc.**

Fault or defect → **Indicate with 'X'**

Line manager → **Driver's, User's or Owners Signature**

Drivers details → **Driver's Number**

Workshop Only → **Workshop Staff Signature**

Defect Report Acknowledged → **Controlling Manager's Signature**

Investigation / Action Taken → **Workshop Staff Signature**

Rank → **Rank**

Date → **Date**

Please tick as appropriate

☐ Defect rectified ☐ No fault found ☐ Temporary repair ☐ Booking confirmed

* (tech init)
Where appropriate all defects must also be recorded in log book P1061



Parcelforce Fault Reports PMT1

The Fault Report (PMT1 or PFMT1) is an integral element of a vehicles maintenance records. Each selection of the document must be completed.

- When completing the form, the operator is responsible for entering the details of the vehicle.
- Give as much detail as possible in the description of defect section, so the technicians don't waste time trying to find the fault.
- Make sure you enter the serial no in the logbook, technician must sign to say work has been completed.

PARCEL-FORCE WORLDWIDE **VEHICLE DEFECT REPORT** Serial No. **573221**

TO BE COMPLETED BY THE DRIVER/USER

Enter Speedo Reading Here. Not if Trailer

SERIAL NUMBER..... SPEEDO READING.....

DEFECT REPORT ISSUING DEPOT: **NAT HUB COVENTRY** DATE..... TIME.....

DESCRIPTION OF DEFECT

Enter 7 Digit Serial No

Enter Todays Date & Time

ENTER FULL DESCRIPTION OF DEFECT

VEHICLE DEFECT MUST BE ENTERED IN VEHICLE CONDITION RECORD BOOK

DRIVERS SIGNATURE..... **You Sign Here** PRINT NAME..... **You Print Here**

TO BE COMPLETED BY THE LINE MANAGER

DEFECT REPORT ACKNOWLEDGED **GET LINE MANAGER TO FILL IN THIS PART**

SIGNATURE..... DATE..... TIME..... HOURS

VEHICLE REMOVED FROM SERVICE

WORKSHOP NOTIFIED DATE..... TIME..... HOURS

VEHICLE PRESENTED TO THE WORKSHOP STAFF: DATE..... TIME..... HOURS

TO BE COMPLETED BY REPAIRING WORKSHOP

NB. CHECK IF VEHICLE/TRAILER IS DUE SCHEDULED WORK

DEFECT RECTIFIED AND VEHICLE CONDITION RECORD BOOK CLEARED

SIGNATURE of REPAIRING TECH TECH CODE GRADE DATE

OPERATOR NOTIFIED VEHICLE READY FOR SERVICE DATE..... TIME..... HOURS

DEFECT REPAIR CLASSIFICATION:

***DELETE WHERE NECESSARY**

WORKSHOP MANAGER / LTT

VEHICLE DEFECT REPORT'S COPY FORWARDED TO DEPOT

SIGNATURE DATE



Introduction – techniques and attitudes

As a Large Goods Vehicle (LGV) driver for Royal Mail, you will have special responsibility not just to yourself but also to all other road users. As a professional LGV driver you will be expected to set an example to other drivers by ensuring that your vehicle is driven at all times with the utmost safety, courtesy and consideration for all other road user.

To become an LGV driver you must possess a high degree of skill in the handling of your vehicle. You must also be prepared to make allowances for the behaviour of others. The right attitude and approach to your driving, together with sound knowledge and the ability to apply professional driving techniques, are all essential ingredients for safe and successful LGV drivers.

To drive an LGV you must first appreciate the differences between driving large vehicles and driving smaller commercial vehicles.

These are:

- Weight.
- Width.
- Length.
- Height.
- Distance needed to bring the vehicle to a halt.
- Distance needed to overtake.
- Control needed when going down hill.
- Power needed to climb hills.
- The need to avoid any sudden changes of speed or direction.
- The need for more forward planning to drive vehicle safely.





Coupling and Uncoupling Articulated Vehicles

Always ensure coupling and/or uncoupling is carried out at a legal and safe location. *See RM SSoW for coupling and uncoupling.*

BEST PRACTICE FOR COUPLING/UNCOUPLING TRAILERS

Coupling

Check you have selected the correct trailer. Ensure that it is in service and MOT and the park brake is applied.

When picking up a trailer from a loading bay only do so, on a Green light.

Reverse towards trailer and stop short. Alight from cab and do primary checks (tyres, body condition etc.)

Ensure that the fifth wheel is below the rubbing plate. Use ECAS system (the suspension raise & lower box at the side of drivers seat) if necessary.

Reverse slowly so that fifth wheel is just under the trailer and raise unit using the ECAS so that the landing legs are clear of the ground continue to reverse until the fifth wheel engages/locks, hearing a clunk.

To confirm that the fifth wheel has engaged and locked give two sharp pulls forward, using a low gear. (Pull test)

If all is well apply unit park brake, switch off engine and remove keys.

Alight from cab, fit the fifth wheel locking device, Dog Clip, connect air and electrical hoses, most of our units have been designed to use the access steps on the nearside. Wind up landing legs and stow handle.

Release the trailer park brake. **IN THE UNLIKELY EVENT OF A ROLL AWAY, IMMEDIATELY DISCONNECT RED AIRLINE, IF SAFE TO DO SO, & ALERT OTHERS BUT DO NOT ATTEMPT TO ACCESS THE CAB OR CATWALK TO APPLY THE BRAKE.**

Where appropriate, ensure dock seal is fully deflated and traffic light is still on green. Slowly pull away from bay/dock check loading of trailer close door/s.

Fit number plate and check operation of all lights.

Uncoupling

Ensure that the ground is firm and level

Always ensure that the area you are attempting to reverse into is clear, if dropping trailer on a bay remove number plate, before docking and ensure a green light is showing.

When you have finished your manoeuvre apply unit park brake and remove the key.

Apply trailer park brake, lower landing legs to make slight contact with ground disconnect airline and electrical hoses these must be stowed correctly to prevent any damage.

Remove locking device, Dog Clip, and release fifth wheel.

Ensure the bay light has remained green, raise unit suspension and move forward slightly to disengage fifth wheel and stop, lower suspension to allow legs to make contact with ground and drive away remembering to raise ECAS/Suspension to normal position when clear of trailer.

Tail Lifts

Safety DOs

Before operating

- Ensure that you have been trained in the safe operation of the tail lift of any vehicle you drive by a competent person.
- Follow the instructions on notices affixed to the vehicle.
- Ensure the vehicle is on firm, level ground if possible and the parking brake is applied fully.
- Keep the immediate area free of people and obstructions.
- Keep all loads stable and secure. Ensure the load is safely on the platform before operating the tail lift.
- Switch off the isolator switch (in cab) before leaving the tail lift unattended.
- Remove your vehicle keys before you leave the immediate area.

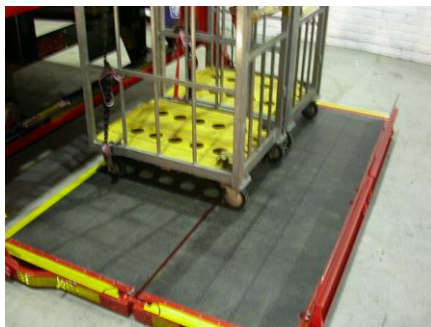
While operating

- Use roll stops, posts or ramp plates with wheeled containers.
- Ensure the load is distributed evenly on the platform and will not snag on the rear of the vehicle during lift/lower operations.
- Keep your feet clear of the platform edges and mind your head on the rear frame.
- Hold onto the hand grips, load or container, NOT the column channels or other working parts.
- Keep hands and arms clear of support arms (if fitted) when the platform begins to close (eg. CAM closure lifts).
- Always try to push (rather than pull) loads onto and off the tail lift platform whenever possible.

Tail lifts are only designed to be loaded with a maximum of **two Yorks** at any one time.

When loading Nested Yorks, the Yorks must be assembled whilst being loaded with the tail lift and then broken down into their Nested configuration once inside the vehicle. **Nested Yorks must not be loaded using the tail lift.**

Load or unload Yorks to the offside of the tail lift platform (as pictured), so as to maximise space available for the operator.



Loads must not exceed the tail lift Safe Working Load (SWL) of 1250kgs for Rigid Vehicles and 1500kgs for Trailers.

Keep clear of the edges of the platform at all times



Loading and Strapping Training Brief

Only trained staff will be allowed to operate the bay doors & dock leveller.

All staff in RM Network including Agency Drivers

The Road traffic Act 1988 states that 'it is an offence to use a vehicle, or cause or permit it to be used, when the position or distribution of its load or the manner in which it is secured means that there is a danger of injury to any person. This carries a maximum fine of £5000 together with obligatory licence endorsements and discretionary disqualification. Case law has clearly established that a load does not actually have to be shed for an offence to be committed. It is also possible for more serious offence of insecure loading to be treated as 'Dangerous driving', a conviction which can result in imprisonment and a substantial fine as well as obligatory disqualification and licence endorsement.'

Health and Safety legislation places the responsibility for health and safety upon the entire workforce. This means that the person loading a vehicle can be deemed responsible for the load in certain circumstances such as, parked-off loads where the driver does not have access to the load before taking the vehicle onto the public highway. It also places a responsibility upon the business to monitor compliance in all aspects of health and safety and to challenge non-compliance where identified.

LOADING AND STRAPPING Rigid Stackable Containers (RSC)

WHERE THERE IS TRAILER CAPACITY, ALL RSC's ARE LOADED ONE ON TOP OF THE OTHER – LIGHT TO TOP – HEAVY TO BOTTOM

THE RSC's SHOULD BE STACKED SAFE AND SECURE

THEY SHOULD BE IN A GOOD SOUND CONDITION
FREE FROM ANY PROTRUSIONS OR JAGGED EDGES

NO RSC GATES SHOULD BE OPEN

ANCHOR POINTS ON LOADS SHOULD BE WELL FORWARD OF
THE LOAD EDGE TO REDUCE SIDE MOVEMENT

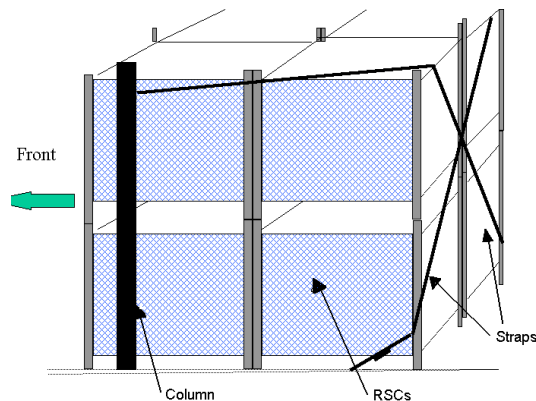
SECURING STRAPS SHOULD BE APPLIED TO EVERY BLOCK OF 4 SINGLE OR 8 DOUBLE STACKS OF RSC's

YOU NEED A FULL SET OF 12 STRAPS TO CARRY A FULL LOAD

REMEMBER
PLENTY OF TENSION SHOULD BE APPLIED

LOADING 14ft DOUBLE STACK CURTAIN TRAILERS

-SIDE VIEW-



THE TOP STRAP COMES OVER THE OUTSIDE EDGES AT THE CENTRE WHERE THE TWO LOADS MEET
AND THEN COMES INSIDE THE FRONT POST OF THE TOP RSC
CROSSING OVER AND GOING ROUND THE BOTTOM OF THE LOWER OPPOSITE STACK
MAKING AN 'X' SHAPE AT THE FACE OF THE LOAD

NOTE HOW THE BOTTOM STRAP ANCHOR POINT IS WELL FORWARD OF THE REAR EDGE OF THE LOAD

LOADING BOX TRAILERS / VEHICLES

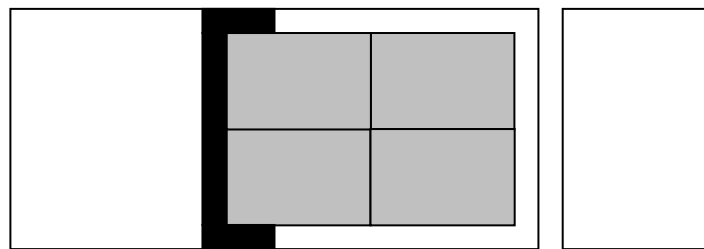
RSC's ARE LOADED IN BLOCKS OF 4 OR 8
THE TOP AND BOTTOM STRAPS COME ACROSS THE FACE OF THE LOAD AND THE ANCHOR POINT IS WELL FORWARD OF THE REAR EDGE OF THE LOAD

- PLENTY OF TENSION SHOULD BE APPLIED -

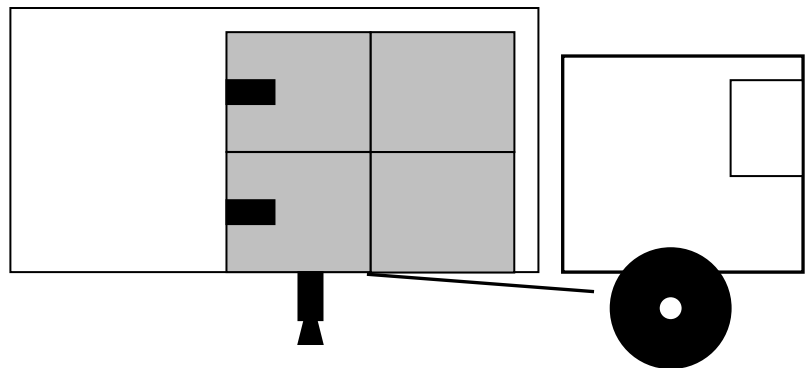
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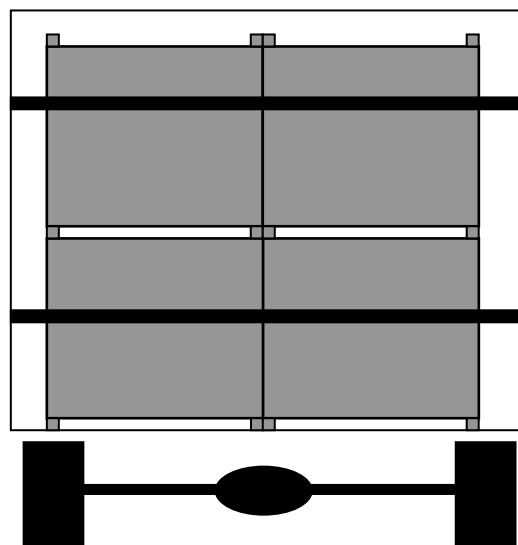
OVERVIEW:
BLOCK OF 4 LOADING
AND STRAPPING
(STRAP IS BLACK)



SIDE VIEW:
BLOCK OF 4 DOUBLE
STACK = 8 RSC's



-REAR VIEW-

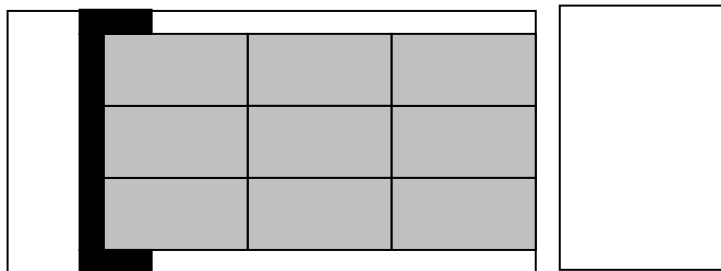


LOADING & STRAPPING YORKS

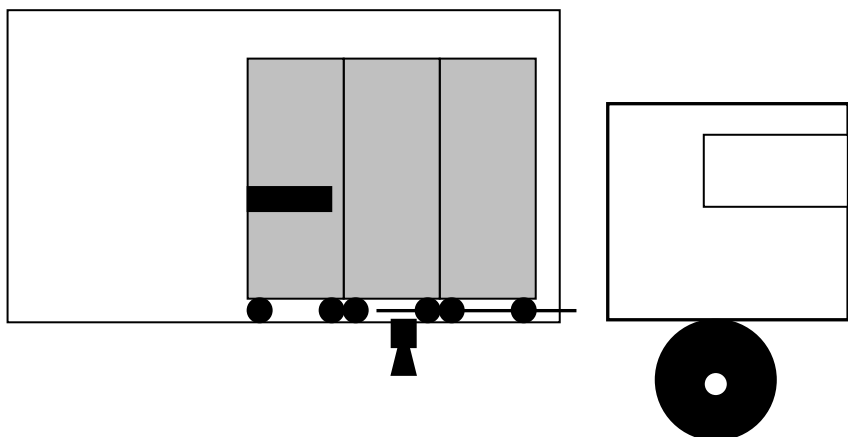
- Fore & Aft method

1. Reverse vehicle on to dock and deploy the dock leveller. Only trained personnel should use a dock leveller. If a tail lift is fitted it will be necessary to lower the tail lift and reverse it below the dock leveller.
2. Check that vehicle is fitted with sufficient number of serviceable load restraint straps (6 STRAPS ON A BOX TRAILER).
3. FIRST YORK - Using the yellow handle, push the loaded York over the dock leveller into the vehicle and park it against the headboard. Position it centrally from vehicle sides (Fig 2). Ensure the brake is applied.
4. SECOND & THIRD YORK- position it either side of the first York. This should leave a space of approximately 200 mm between the sides of the curtains/box and the Yorks.
5. Repeat steps 3 & 4 for additional Yorks until three rows (9 Yorks) have been placed in the vehicle. (See figure below)
6. When York containers are not available in multiples of three i.e. 11 Yorks, secure as one block of 9 and one row of 2, if possible complete any shortfalls with empty Yorks.
7. Unhook the lower strap from its park position. Locate the strap round the rear of the Yorks, follow across to the opposite side and hook the strap onto the rail on the opposite side
 - PLENTY OF TENSION SHOULD BE APPLIED -

OVERVIEW:
BLOCK OF 9 LOADING
AND STRAPPING
(STRAP IS BLACK)



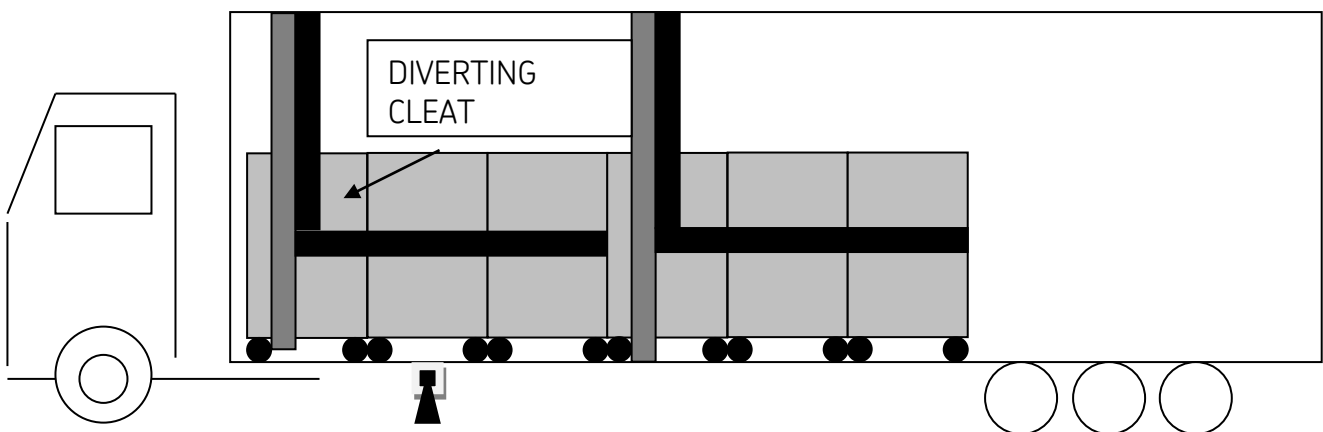
SIDE VIEW:
BLOCK OF 9 yorks



LOADING 14ft DOUBLE STACK CURTAIN TRAILERS WITH YORKS

1. Reverse vehicle on to dock and deploy the dock leveller. Only trained personnel should use a dock leveller. If a tail lift is fitted it will be necessary to lower the tail lift and reverse it below the dock leveller.
 2. Check that the vehicle is fitted with a sufficient number of serviceable load restraint straps (6 each side). Check that support posts are located in their correct upper and lower positions and that the straps are in the parked position ready for loading, i.e. Hooked on to front of the next rearward support post.
 3. Load the Yorks using the procedure described in steps 3 to 6 in previous section.
 4. Unhook the strap from its park position and route the strap around the lower strap-diverting cleat (figure 5), ensuring that the strap remains in position and the buckle is in the 'free' mode. Pull the strap round to the approximate centre on the middle York, repeat for other side.
 5. Hook the straps into the sewn in ring on the opposite webbing (both). Tighten the strap and lock using the over centre buckle. One strap buckle will face inwards, this should be locked in the approximate centre of the Yorks and all tightening should be carried out only on the strap that is rear facing. (See following picture)
 6. Not all vehicles will be equipped with sewn in webbing rings, it is acceptable to use hook to hook for locating the straps.
- Repeat steps 3 to 5 for additional Yorks.

Fig 1



This example shows the correct location for the buckles and final position of the straps.

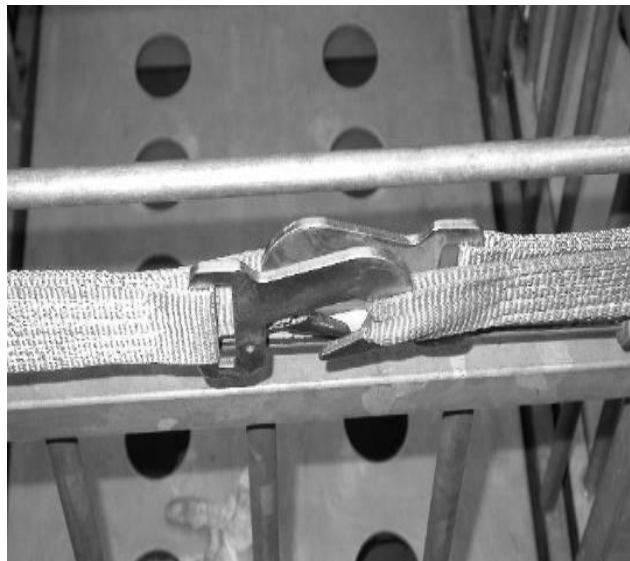
DIVERTING CLEAT



FINAL POSITION



The next picture shows the webbing type retainers, please note however, it is acceptable to use the hooks directly together on vehicles not equipped with the orange webbing loops.

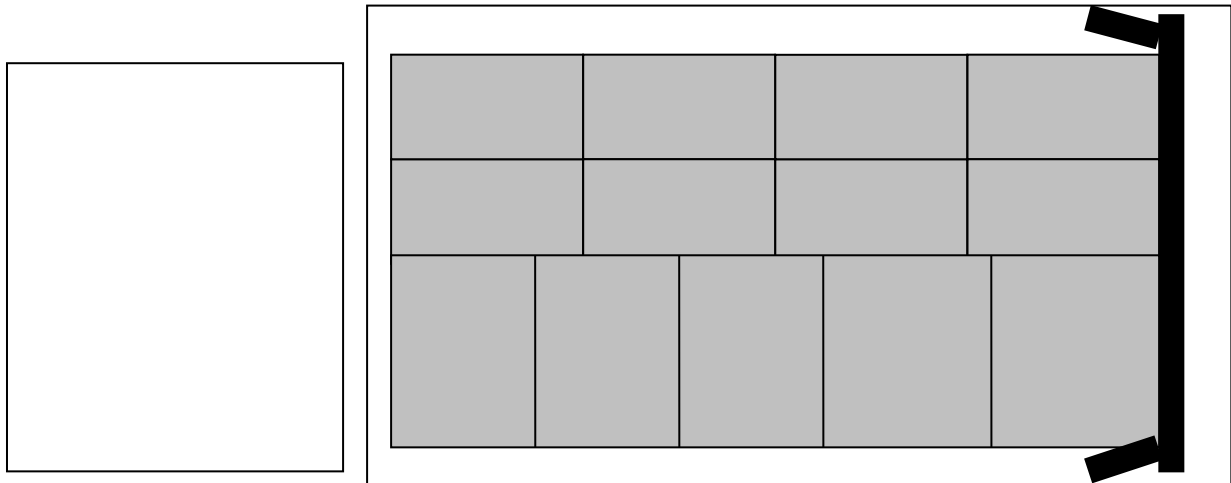




BLOCK OF 13 BUILDS FOR YORKS ON A BOX TRAILER

This enhanced load plan enables an extra 4 Yorks to be loaded compared to the conventional fore and aft method allowing a total of 49.

1. Reverse vehicle onto the dock and deploy the dock leveller. Only trained personnel should use a dock leveller. If a tail lift is fitted it will be necessary to lower the tail lift and reverse it below the dock leveller.
2. Check that the vehicle is fitted with a sufficient number of serviceable load restraint straps. If using a curtain side trailer ensure it is fitted with lowering cleats. Check that the support posts are located in their correct upper and lower positions and that the straps are in the parked position ready for loading. i.e. Hooked onto the front of the next rearward support post.
3. To ensure an even load spread, place the heavier Yorks along the centre of the trailer i.e. positions
4. FIRST YORK - Using the yellow handle, push the loaded York over the dock leveller into the vehicle. Once at the headboard turn it to face the right hand wall of the trailer leaving 100mm between the front of the York and the trailer sidewall. Ensure the brake is applied.
5. SECOND YORK- position it next to the first York turning to face the right hand wall of the trailer leaving 100mm between the front of the York and the side of the trailer.
6. THIRD & FOURTH - Wheel the next York to the front of the trailer parking it against the headboard to the left of the other Yorks. Position the next York to the left of the previous one, which should leave approx 100mm between the side of the York and the trailer side.
7. Load the remaining Yorks using the sequence in figure 3, which will give you a block of thirteen.
8. Unhook the strap from its park position and route the strap around the strap-diverting cleat, (figure 5 when using a curtain sided trailer) ensuring that the strap remains in position and the buckle is in the 'free' mode. Locate the strap round the rear of the Yorks, follow across to the opposite side and hook the strap into a floor receiver forward of the load. When loading a box trailer, hook the strap into the opposite load track. Tighten the strap and lock using the over centre buckle
9. When using a curtain sided trailer, Repeat with the opposite side load restraint strap.
10. When York containers are not available in blocks of 13, if possible complete any short falls with empty Yorks. Alternatively revert to the conventional fore and aft method.



Unloading trailer using a dock leveller – block of 13 method

The Yorks should be unloaded in the reverse order, using any available free space to rotate Yorks slightly when necessary to release any bags that have become caught between the containers.

You must ensure that the load security straps, once released, are correctly stowed in their park position.



Trailer Heights

Royal Mail use trailers of various heights, to safeguard your vehicle ensure you set the in-cab height meter to the combination height of your unit and trailer before you leave the yard.

Single Stacking Box – 13' 4"



Remember your unit may be taller than your trailer, Our Royal Mail MAN units run at 13' 5"

WARNING
Set in cab adjustable height indicator to solo tractor unit height.
13' 4" (4.08Metres)

Double Stacking Box or Curtain Side – 14'



WARNING
Combined Overall Height =
14' (4.24Metres)

Old 75 York Double Deck – 14' 7"



WARNING
COMBINED OVERALL HEIGHT-
14' 7" (4.44 Metres)

New 75 York Double Deck – 14' 11"



WARNING
COMBINED OVERALL HEIGHT
14' 11" (4.55 Metres)

92, 95, 98 York Double Deck – All run at 16' 2"



All white fronted trailers run at:

WARNING
COMBINED OVERALL HEIGHT
16' 2" (4.92 Metres)

Remember if you exchange trailers in your duty check and reset height meter

Set your in-cab height meter



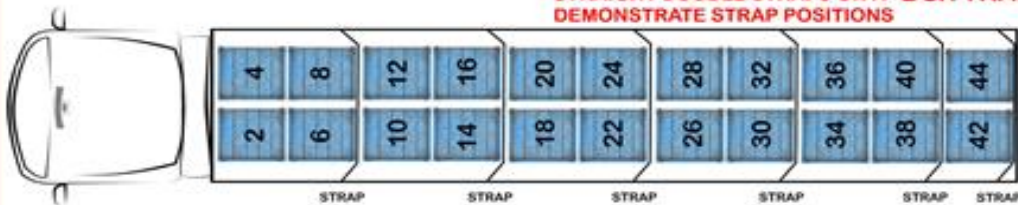


RSC & FSC LOADING PLAN

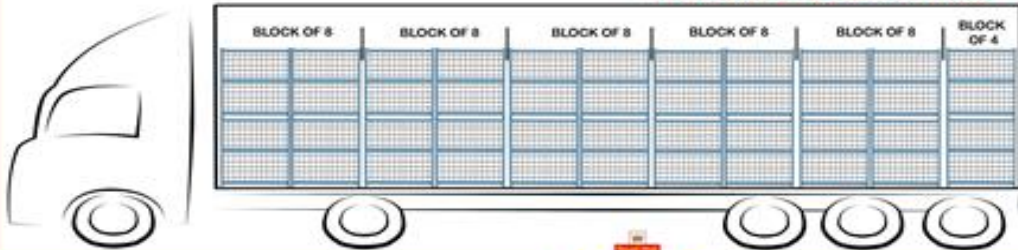
BOX OR CURTAIN SIDER WITH
44 EMPTY RSCs OR FSCs DOUBLE STACKED

PART ROWS TO BE STRAPPED SAFELY

STRAIGHT DOUBLE STRAPS ON A "BOX TRAILER" - BLUE LINES
DEMONSTRATE STRAP POSITIONS



CROSS STRAPS ON A "CURTAIN SIDER" - BLUE LINES
DEMONSTRATE STRAP POSITIONS



SWDC

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44 EMPTY RSCs OR FSCs DOUBLE STACKED ON A BOX OR CURTAIN SIDER



YORK LOADING PLAN

BOX OR CURTAIN
SIDER WITH
49 EMPTY
ASSEMBLED
YORKS

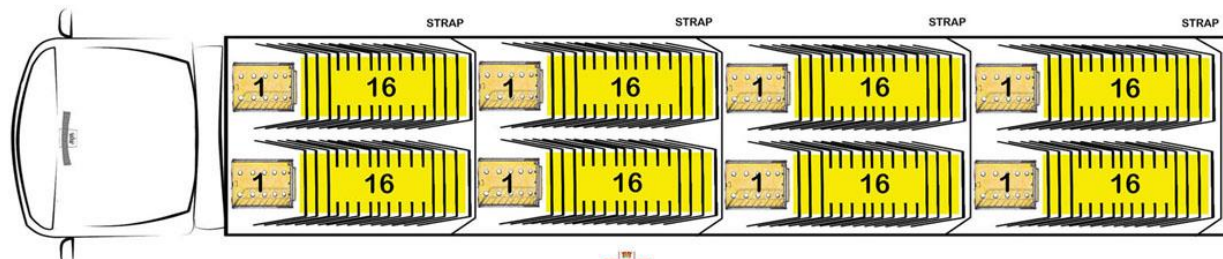
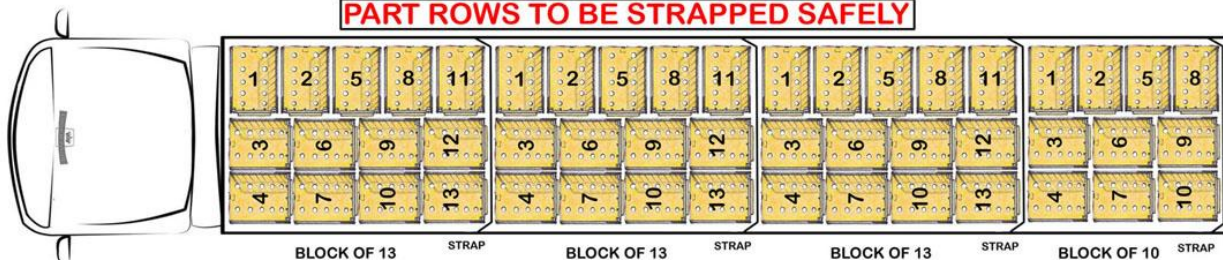


Apply the brake
to each
assembled york
and the last
york of a nest

BOX OR CURTAIN
SIDER WITH
136 EMPTY
NESTED
YORKS



PART ROWS TO BE STRAPPED SAFELY



SWDC

1

EMPTY AND NESTED YORKS ON A BOX OR CURTAIN SIDER

YORK LOADING PLAN

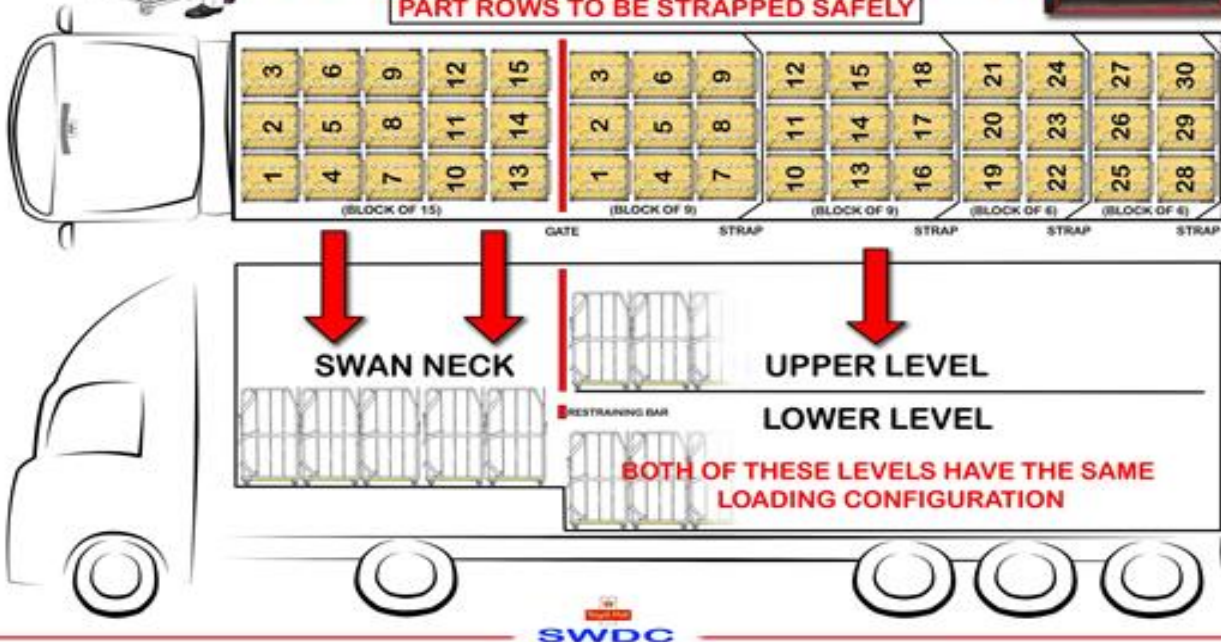
Apply the brake to each assembled york

75 DOUBLE DECKER WITH
75 EMPTY ASSEMBLED YORKS

PART ROWS TO BE STRAPPED SAFELY



75 EMPTY ASSEMBLED YORKS

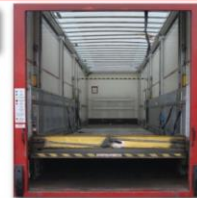


YORK LOADING PLAN

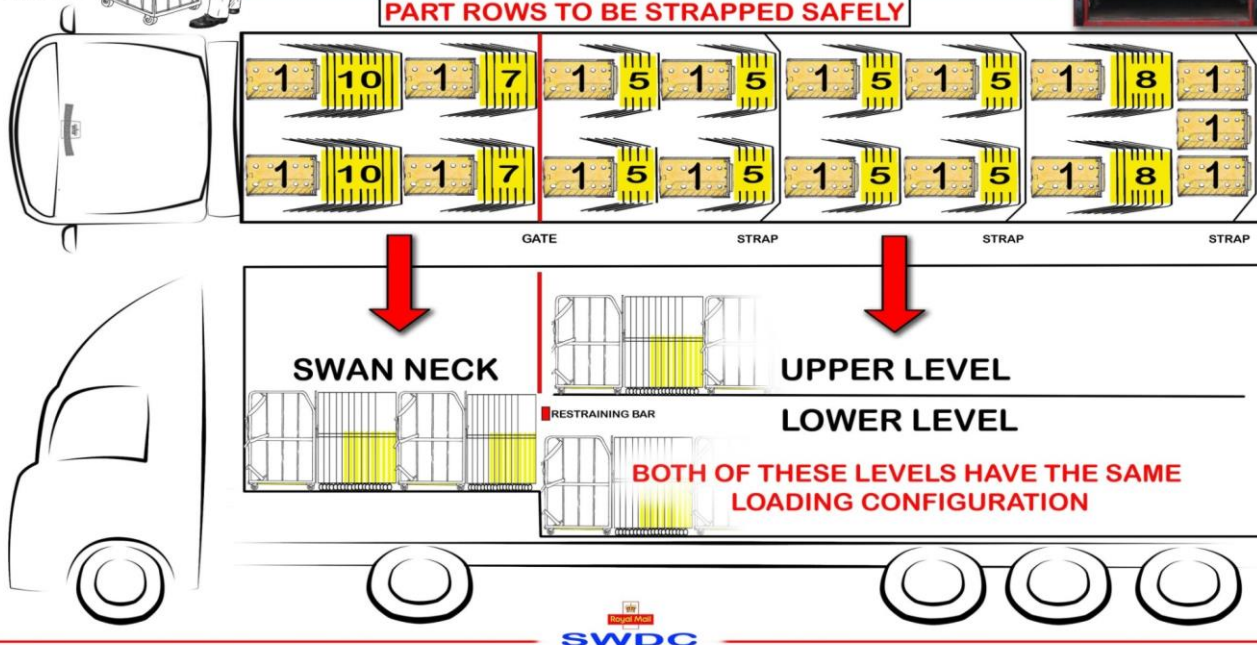
Apply the brake to each assembled york and the last york of a nest

75 DOUBLE DECKER WITH
146 NESTED AND 30 EMPTY ASSEMBLED YORKS
(176 in total)

PART ROWS TO BE STRAPPED SAFELY



146 NESTED YORKS AND 30 EMPTY ASSEMBLED YORKS (176 total)



YORK LOADING PLAN

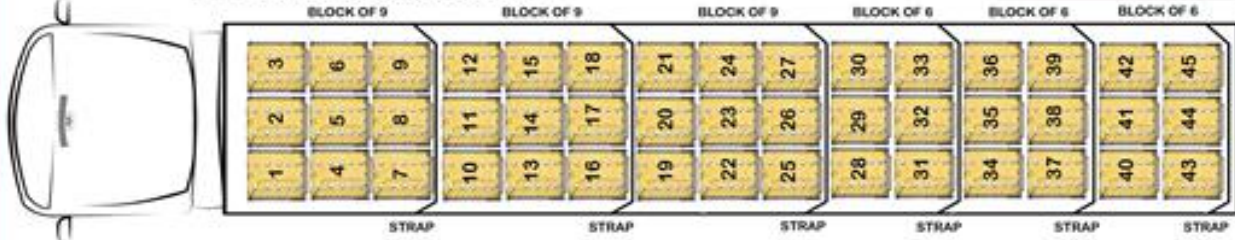


92 DOUBLE DECKER WITH
92 EMPTY ASSEMBLED YOKES

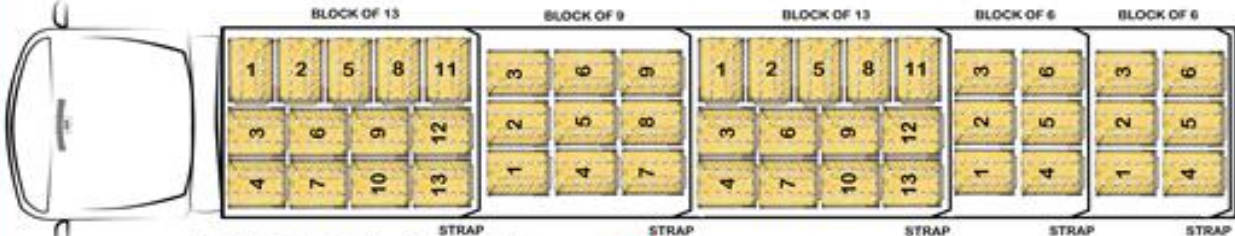
PART ROWS TO BE STRAPPED SAFELY



UPPER DECK: 45 YOKES



LOWER DECK: 47 YOKES

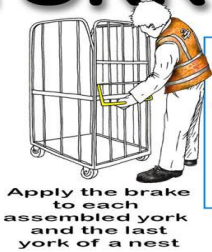


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4

92 EMPTY ASSEMBLED YOKES

YORK LOADING PLAN

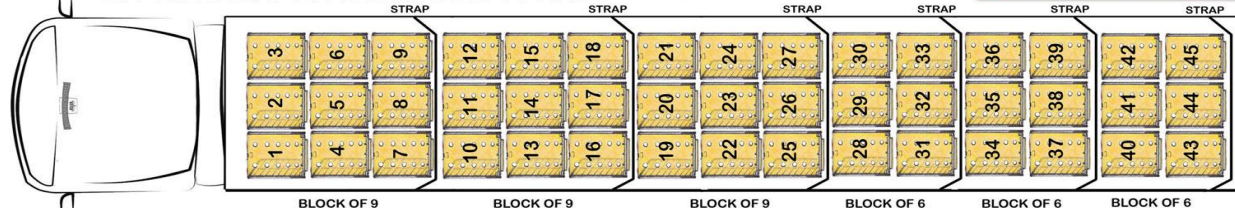


92 DOUBLE DECKER WITH
136 NESTED AND 45 EMPTY ASSEMBLED YOKES
(181 IN TOTAL)

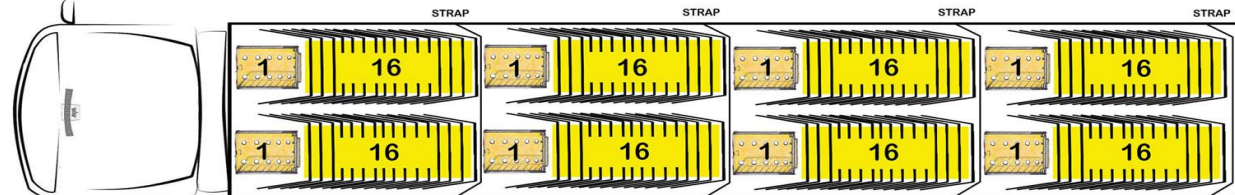
PART ROWS TO BE STRAPPED SAFELY



UPPER DECK: 45 ASSEMBLED YOKES



LOWER DECK: 136 NESTED YOKES



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136 NESTED AND 45 EMPTY ASSEMBLED YOKES (181 TOTAL)

Load Plan For Cartwright Dual Use 95 York Carrier Upper Deck

10 Yorks				13 Yorks				13 Yorks				10 Yorks				FRONT
REAR	10	7	4	13	10	7	4	13	10	7	4	10	7	4		
	9	6	3	12	9	6	3	12	9	6	3	9	6	3		
	8	5	2	1	11	8	5	2	1	11	8	5	2	1		

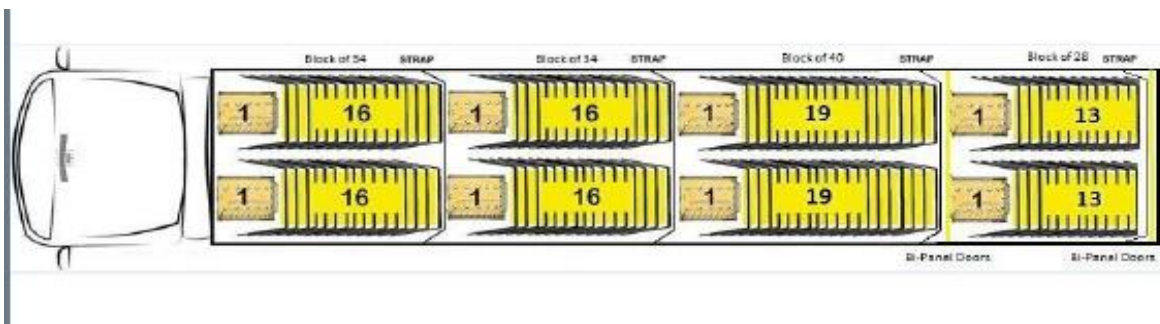
13.6m Straight Frame Double Deck - Lower Deck - With Internal Rubbing Rails (Trailer Internal Dimensions: Width 2378mm x Length 13305mm) Moving Deck - Width 2265mm x Length 13067mm

Load Plan For Cartwright Dual Use 95 York Carrier Lower Deck

13 Yorks					13 Yorks					13 Yorks					10 Yorks					FRONT
REAR	12	10	7	4		13	10	7	4		13	10	7	4		10	7	4		
	13	9	6	3		12	9	6	3		12	9	6	3		9	6	3		
	11	8	5	2	1	11	8	5	2	1	11	8	5	2	1	8	5	2	1	

13.6m Straight Frame Double Deck - Lower Deck - With Internal Rubbing Rails (Trailer Internal Dimensions: Width 2378mm x Length 13305mm) Moving Deck - Width 2265mm x Length 13067mm

Note: To achieve maximum load stability it is essential that the heaviest Yorks are placed on the bottom deck keeping the majority of weight as low as possible to counteract the weight of the Yorks on the moving deck. Where numbers allow ensure that the heavier Yorks are loaded along the centre of the trailer.





PARCELFORCE Operations

95DD Dual Use Trailer - Basics to Ensure Safe Travel



Ensure travel with three pin trickle charge lead attached.
Only attach Anderson lead if the trailer brake is on and the trickle charge removed.



The inner Deck Doors must be closed and no parcels' to be visibly above these doors whatever position / level the upper deck is in.



If the deck is in its highest position then below deck must be fully loaded up to the Red line marked around the trailer of the lower deck and must be more than $\frac{3}{4}$ of the way full of the length of trailer (tapered down towards rear), i.e. full or very nearly full. stopping 2 feet from the back door.



If the top deck is in this position (the 1 metre height auto stop position) then there must be parcels below it.
If there are no parcels below the upper deck then the upper deck must be lowered to as shown in the second diagram.

YORK LOADING PLAN



Apply the brake to each assembled yoke

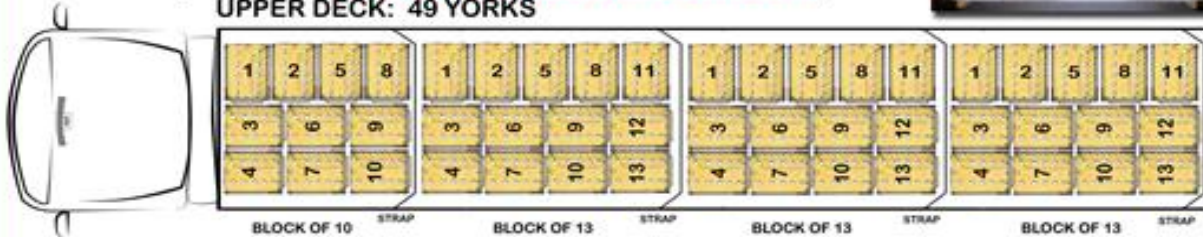
98 DOUBLE DECKER WITH
98 EMPTY ASSEMBLED YOKES

PART ROWS TO BE STRAPPED SAFELY

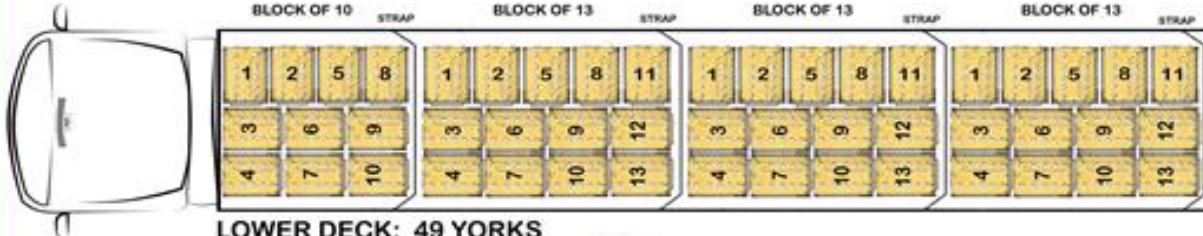


92 EMPTY ASSEMBLED YOKES

UPPER DECK: 49 YOKES



LOWER DECK: 49 YOKES



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YORK LOADING PLAN

Apply the brake to each assembled yoke and the last yoke of a nest



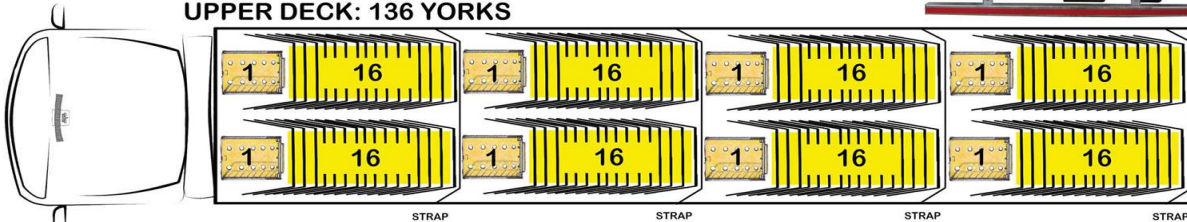
Cartwright
98 DOUBLE DECKER WITH
256 NESTED AND 16 EMPTY ASSEMBLED
YOKES
(272 IN TOTAL)

PART ROWS TO BE STRAPPED SAFELY

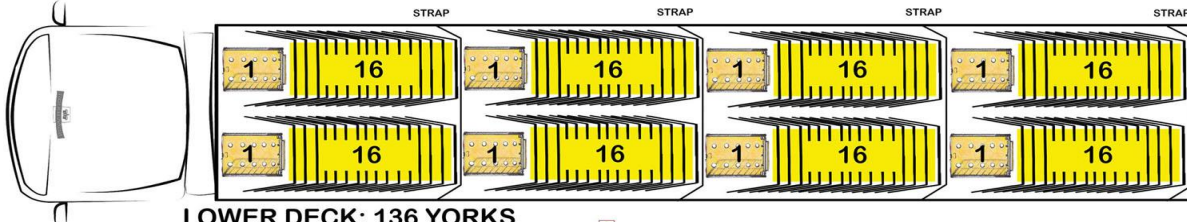


256 NESTED AND 16 EMPTY ASSEMBLED YOKES (272 TOTAL)

UPPER DECK: 136 YOKES



LOWER DECK: 136 YOKES



SWDC

8A



MOVE TO TIME POLICY

1. Introduction

The Move to Time policy for Network services has been in existence for some time, but given the recent introduction of Network 2007, the instructions are being reissued to remind everyone of the importance of the policy.

2. The Move to Time Policy

- The over-riding principle of this policy is to maximise the amount of traffic consistently arriving at inward mail centres to schedule. To enable this to happen, any failures at source have to be identified, a root cause analysis performed and any problems fixed within 24 hrs.
- The policy is applicable at all mail centres, RDCs and other network nodes.

These include the following: -

- a) Outward mail centre to hubs
- b) Outward RDCs to hubs
- c) Outward, intermediate and inward network hubs
- d) Inward RDCs to mail centres / delivery offices
- e) Inward mail centres to delivery offices

- Traffic failing to arrive at network hubs to time should not be transferred to due network connections, where this means that the connection will be prevented from leaving to time.
- Where it cannot meet the due network connection, the traffic must be held and forwarded on the next available outlet, (even if this service is a late positioning service for failed traffic).
- No network services must ever be held to connect late arriving vans.
- Any failures due to late running vans must be reported as such. Any hub failures due to other issues are to be recorded as network work-plan failures at the hub.

3. Despatch Standards

- In addition, the policy also says that outward mail centres will present mail to network distribution according to specification, (i.e. on time & within correct segregations), so that all vans depart to the scheduled timings. On no account should vehicles ever be held at MCs due to late despatch. Vehicles should depart to time and any failures reported as MC outward failures.
- Any flex/ad-hoc/sweeper vehicles must operate in advance of the final vehicles, (subject to LCM 16/07) and the arrival time(s) agreed with the receiving hub, (i.e. the receiving hub must be informed of all special/extra/flex vehicles through the LCM 16/07 process).
- Unadvised and unauthorised vehicles may be returned to despatching mail centres and failures will be recorded as MC outward failures).
- No additional vehicles will be accepted at network points after the scheduled "final" vehicle is due to arrive.
- All parties must agree any changes to the vehicle schedules, the network model and Service Level Agreements (SLAs). This is done through the Network Change Application Process LCM 28/06.



- LCM 28/06 and 16/07 can be obtained by following this Intranet link. Letter Circulation Minutes

4. Despatch Time

- For clarity, the despatch time from the Mail Centre is the time that the vehicle wheels start moving off the loading bay. The completion of waybills etc. must be completed prior to the despatch time. Generally this process should take no more 2-3 minutes and should be agreed locally.

5. Hold codes (Central Postal Control)

- Only Central Postal Control shall have the authority to hold transport beyond its scheduled departure time(s).
- CPC continue to have the responsibility for the day-to-day running of the Move to Time policy and will have the complete picture of the cause and effect of any decision to hold a service.
- This may be done for reasons out of Royal Mail control or for the deployment of established contingency plans, (which they CPC have the sole authority to deploy).
- If any part of the pipeline considers a hold necessary, CPC must be contacted on 5785 2999. In no circumstance must a vehicle be held without a code being issued by CPC or held in anticipation of getting a hold code.
- Failure to get a hold code will result in the Nerve Centre being informed of a failure to be attributed to the Mail Centre or Hub. All hold codes will be shown on the National Network Report each morning.

Below is the breakdown of a typical hold code.

The section in Green indicates how long a driver can be held.

CPC Identification Number

Number of Minutes a code is valid for

29598 **NGH** **015**
Initials of issuing officer



Waybills

- Royal Mail Business policy is to carry a waybill, but some offices are paperless.
- You must obtain a waybill prior to departure from Bay Staff or Traffic Office.
- Trailer swaps the waybill must be handed to the second leg.
- Hand in the waybill at the point of destination.
- Taken to the bay manager / bay controllers or traffic office.

Customer Collection Receipts (CCRs)

- Mails Verification starts with each collection of mail, you should ensure that the customer gives us the following items.
- A Customer Collection Receipt (CCR)
- A Posting Cheque / Confirmed OBA Sales Order – (OBA – Online Business Account)
- A copy of the Line Listing for their mailing.
- A copy of the Computer Planning Report for their mailing.
- Cage Cards should be attached to the RSC's / Yorks.

Waybill *Please fill in this form giving as much information as possible* 

				Departure Time				
Waybill Number	Linehaul Number	YMS Code	Duty Number	Date	Scheduled	Actual		
RM000125	RM.AA.X1			24/05/2006	16:13	16:13		
Despatched From			Despatched To					
Romford Q.E. House Sandgate Close Romford RM7 0AB			BAIRSTOW EVES N/A RM13					
Transport Type		Registration Number		Trailer Number				
7.5 TONNE RIGID				A123CDE				
CONTAINER QUANTITIES								
	M1/P1	M2/P2	M3/B	ST1C	ST2C	MOR	D2D	EMPTY
RSC's:	0	0	0	0	0	0	0	0
Yorks:	0	0	0	2	0	0	0	0
TOTALS		Received From:		No of Yorks		No of RSCs		
RSC's:	0			Filled	Empty	Filled	Empty	
Yorks:	2							
NOTES								
Despatched by				Driver				
Name		Signature		Name		Signature		
Received by				Arrival time				
Name		Signature		Scheduled		Actual		



Waybills

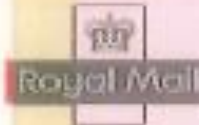
Customer Collection Receipt

CCR Number

B153951

Date

6.7.2010



A Customer Collection Receipt should accompany each collection made. Line Listings and Computer Planning Reports should be given to the driver prior to / or with the first collection for any new job.
'Posting cheques for each job should be sent in daily with, at the latest, the last collection'.
Poster to please complete the date, the Royal Mail Address, Section 1, Section 2 inc Scheduled and Actual collection time, Section 3.
Driver to please check the number of cages is correct, complete the arrival time on the dock at the DC and all of Section 4.

Royal Mail Address - (where mail is being taken to)

S.W.R.D.C.
BRISTOL.

APPENDIX 4

1. Poster Details

Poster address

D.V.L.A.
MORRISTON.
SWANSEA. SA5. 7XX.

Contact name

J. BLOGGS

Reference Number

Telephone number

01234 56789

2. Collection Details

Scheduled Collection Time

15.00

Actual Collection Time

14.50

Total cages/
yorks Collected

10

Total cages/
yorks shut out

NIL

Drivers arrival time
at DC

Job Name/Title	Job Ref Number	Account Number	Format	Product & Class	No. Containers	Shut Outs	ZM	CPR Provided	Use Listing Provided	Posting Cheque Provided
V95	1234	10987654	LL	M3	10			✓	✓	✓

3. Poster's Comments

Poster Representative signature

J. BLOGGS

4. Driver's Information

Posting Cheques CPR Line Listings Miscellaneous

Duty Number

007.

Driver's Name (Please Print)

A. DRIVER

Driver's Signature

A. DRIVER

Data Capture
Complete (Initials)

Date

20

CUSTOMER COPY (void)

P4571/September 2000
394506



Parcelforce Waybill

Destination: COVENTRY LD

Duty #: CVNH608

Departure Time: 2016-06-01 19:26

Trailer: 4335028;PFW

Bay: 302

Seal #: 2025936

Fill%: 100

Load Type: OUT - Trl

Product Type: LOADED

Vehicle Type:

*****STOP ! *****

Being given your waybill
does **NOT** mean your trailer is ready.

ALWAYS check the traffic lights first.

NEVER CONNECT TO A TRAILER ON A RED LIGHT.
USE DRIVERS OFF SIDE MIRROR TO CHECK TRAFFIC LIGHTS PRIOR TO
CONNECTING AND BEFORE PULLING THE TRAILER OFF BAY.

For Shutdown Issues Phone **02476213429 / 3023**

Arrival Details

--- Date: _____ Time: _____

--- Seal No: _____

--- Manager Signature: _____

Site Rules

- 1 - ENSURE THE TRAILER NUMBER ON YOUR WAYBILL MATCHES THE TRAILER ON THE BAY ALLOCATED
- 2 - NEVER EXCEED THE SITE SPEED LIMIT OF 10 MPH - THIS IS A MANDATORY INSTRUCTION
- 3 - ALWAYS FOLLOW THE ONE WAY SYSTEM AROUND THE YARD
- 4 - CLASS 3 LONG SLEEVED HI VIS VEST OR JACKET AND PPE MUST BE WORN AT ALL TIMES IN THE YARD
- 5 - DO NOT THROW PALLETS, SHRINK WRAP OR ANY RUBBISH INTO THE YARD AREA
- 6 - BE AWARE OF YARD CHECK & MARSHALL STAFF CHECKING TRAILERS IN THE YARD
- 7 - IN THE EVENT OF FIRE PROCEED TO THE NEAREST FIRE ASSEMBLY POINT
- 8 - FOR VEHICLE OR OTHER DRIVER ISSUES PHONE 02476213007 / 3010
- 9 - FOR BAY SHUTDOWN ISSUES PLEASE PHONE 02476213429 / 3023

ALL OF THE ABOVE RULES MUST BE ADHERED TO AT ALL TIMES

PARCELFORCE WORLDWIDE
- DRIVER WAYBILL - COV HUB

Double Deck Trailer Breakdown (Checklist)

This is a small checklist the driver can go through if he finds that the deck will not move.



Ensure the trailer has full air pressure as the early trailers have air over hydraulic deck lock systems.
Has the Anderson lead been plugged in? And the trickle charge lead unplugged? Do not operate the deck with them both plugged in as this will cause the deck system to trip out.



Have you activated the reset switches, by either pushing the reset button or the rocker switch on the Motive Unit, (reset buttons are directly at the rear of the drivers compartment)



Does the Voltmeter (on the front of the trailer) show a charge at or above 24 Volts?
Do the LEDs below the Voltmeter on the front of the trailer both show green? If not, push to reset the circuit breaker.
Are both Red LEDs illuminated, on the right of the voltmeter? If not illuminated when the split charge Susie is connected, this points to a fault.



If the trailer batteries are flat, and all options have failed, a Power pack can be used, via the Anderson socket on the rear of the trailer.



If the deck locks are retracting but the deck won't move there is a proximity sensor override switch fitted at the rear N/S of the trailer which can be used to raise or lower the deck to retrieve the load. This button must be pressed whilst the deck controls are operated and retraction of the locks is confirmed. Note: Once operation is completed the fault must be identified & rectified.

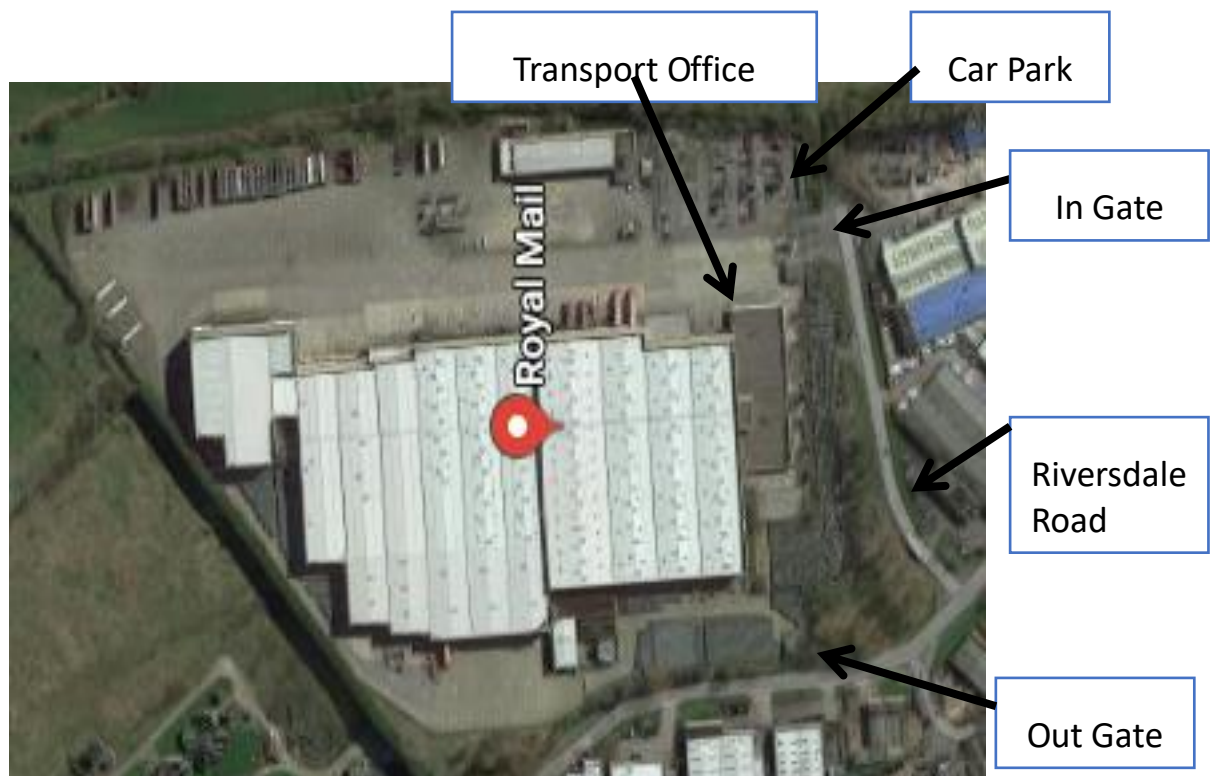
If after checking the above, or you are not sure how to check please call Royal Mail Breakdown on 0345 2660005



VOC or Mail Centre – Location Map



VOC/Mail Centre– Site Plan



Coventry Parcelforce - Location



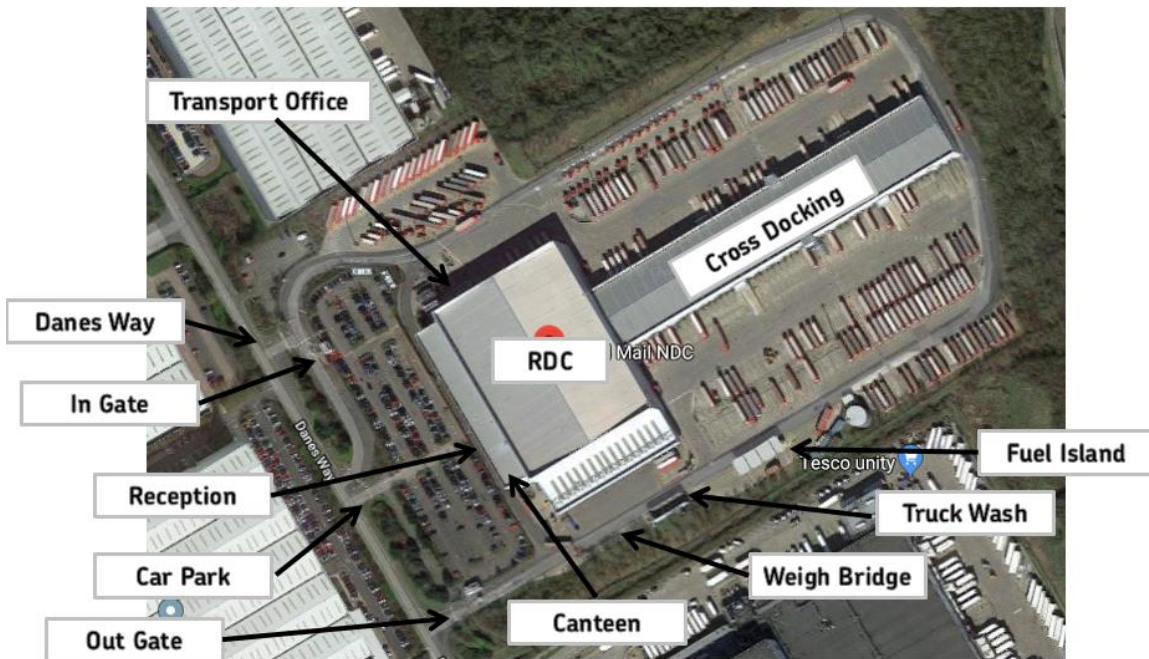
Coventry Parcelforce - Site Plan



National Distribution Centre – Location



National Distribution Centre – Site Plan





Royal Mail Atherstone & NDC VOCs



SAFETY FIRST: Road Safety Campaign 2018

All Royal Mail Drivers have a responsibility to drive safely and minimise risk to themselves and the public. As part of the focus on road safety, we are sharing a series of Road Safety messages with drivers in Logistics; this one is on **Bridge Strikes**.

Background and context

Network Rail statistics show that there are over 2000 bridge strikes every year costing more than £23m. The A5 in Hinckley has been the site of numerous bridge strikes over the years and since acquiring our site in Atherstone, several Royal Mail double deck trailers have been seriously damaged or written off at this bridge.



Facts and Figures

During the peak operating period last year there were three major bridge strikes involving Royal Mail 16' 2" double deck trailers and one involving a 14' 7" 75 York double deck trailer.

The incidents involving the 16' 2" (95 York dual use, double deck trailer) were due to a failure to observe a matrix warning sign and clear static road and bridge mounted signs. The incident involving the 14' 7" (75 York double deck trailer) was caused by a failure to reset the trailer height for travel following the raising of the rear suspension during the loading/unloading process.



Action

- Ensure you set the cab height meter to the maximum height of the unit/trailer combination.
- Do not use the A5 south when leaving Atherstone with a double deck trailer.
- Use the specified route for double deck trailer journeys when travelling to and from Atherstone PSC.
- Observe all road signs and act accordingly.
- Never attempt to pass underneath a bridge signed at a height less than your unit/trailer combination.
- Only use the Isotrak in built Sat-Nav or a Truck Sat-Nav that allows you to set height/weight/length.

Specified Route for Double Deck Trailers travelling to and from Atherstone PSC

All Double Deck Trailer duties must use the following routes:-

Leaving Atherstone PSC - use A5(North) Tamworth-M42

Going to Atherstone PSC - use M42(Jnc 10)-A5(South)



Safety First - Bridge Strikes - NDC, Atherstone Voc 2018

Danger—Low Bridge A5 Hinckley



Danger A12 — Blackwall Tunnel

